

LDĀDESIGN

NEWTONGRANGE TOWN CENTRE REGENERATION MASTERPLAN



FINAL MAY 2018

LDĀDESIGN

Ryden
Property Consultants



**Goodson
Associates**

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THIS DOCUMENT HAS BEEN PREPARED AND CHECKED IN ACCORDANCE WITH ISO 9001:2008

The Wheelhouse Cafe

1

INTRODUCTION

1.0 INTRODUCTION

■ This Report has been prepared on behalf of Midlothian Council in conjunction with its Borders Railway Blueprint Partners and sets out a comprehensive, detailed and appraised masterplan to bring about high quality development within a cohesive, community led vision for Newtongrange Town Centre.

1.1 BACKGROUND

Since the opening of the Borders Railway in September 2015, Midlothian Council and the Borders Railway Blueprint Programme have sought to exploit the opportunities provided by the railway to grow and enhance communities along its corridor. As such, the Newtongrange Town Centre Masterplan is one of four separate, but interconnected masterplan areas along the corridor to benefit from masterplanning of this nature.

Two of these projects fall within Midlothian Council, namely Stobhill/Lady Victoria Business Centre and Newtongrange Town Centre and the other two are being undertaken by Borders Council at Galashiels and Tweedbank.

1.2 BRIEF

The masterplanning brief was to shape an economically viable future for the town centre in the context of the existing local settlement, the wider Midlothian area and the improved connectivity to Edinburgh provided by the railway.

The masterplan area, totalling 2.63 ha and in multiple ownership, consists of Newtongrange Railway Station and the sites of Brodstown Developments Ltd, Newtongrange Leisure Centre and Library, Newbattle Swimming Pool and Newbattle Parish Church.

To fulfil this brief, Midlothian Council appointed a multi-disciplinary design team led by *LDA Design* in May 2017 to complete masterplanning exercise. The full design team included:

LDA DESIGN

Masterplanning | Landscape | Engagement

JTP

Architectural Services | Engagement

RYDEN

Commercial/Property Advice Services | Planning

GOODSON ASSOCIATES

Civil & Structural Engineering

ITP ENERGISED

Environmental Services | SEA | Acoustician | Archaeology

HULLEY & KIRKWOOD

Mechanical & Electrical Engineering Services

A key element of the brief was to engage with a wide variety of stakeholders (landowners and tenants, community council, interest groups, statutory consultees and local councillors) and the wider public in order to understand the range of interests that exist, how these could potentially shape the masterplan itself and the most appropriate spatial strategies moving forward.

In parallel, the design team focussed on analysing the prevailing property market, examining engineering and infrastructure conditions, assessing environmental impacts and undertaking an urban design and architectural review of the town centre to understand the full range of constraints and opportunities within the town centre and to develop the most commercially feasible masterplan solution. The main themes from the “Borders Railway – Maximising the Impact: A Blueprint for the Future” document were also incorporated into the design principles.



¹ MASTERPLAN SITE LOCATION

— SITE AREA

1.3 SUPPORTING WORK

In addition to the outputs of the brief in the form of this Masterplan Report, a number of supporting strands of work have been undertaken:

STRATEGIC ENVIRONMENTAL ASSESSMENT SCREENING OPINION

As part of the design teams environmental work, a Strategic Environmental Assessment 'Screening Opinion' was sought from the Scottish Government by ITP Energised in June 2017 to determine whether there would be significant environmental impacts arising from the masterplan. In July 2017, the Scottish Government advised that there would not be significant environmental affects arising from the masterplan and that a Strategic Environmental Assessment would not be required.

NEWTONGRANGE MASTERPLANS RETAIL STRATEGY

Given the proximity and inter-related nature of the Town Centre and Stobhill/Lady Victoria Masterplans, the design team regularly liaised with the appointed Stobhill/Lady Victoria design team, led by Smith Scott-Mullen. As part of this dialogue, Colliers International and Ryden jointly undertook a supporting Retail Strategy to ensure a cohesive approach to the provision of future retail uses within Newtongrange.

NEWTONGRANGE MASTERPLANS ECONOMIC IMPACT ASSESSMENT

An Economic Impact Assessment has also been commissioned to look jointly at the likely impacts arising from both the Town Centre Masterplan and the Stobhill/Lady Victoria Masterplan for the local area and the Borders Railway Blueprint Programme.

The above assessments along with other studies are summarised in this report and are available in full in the supporting documents.

1.4 STRUCTURE

This Masterplan document is set out in 7 sections as follows:

2 DEVELOPMENT AND REGENERATION CONTEXT – setting the scene in terms of the Borders Railway Blueprint Programme, the wider growth context in both Midlothian and in the Edinburgh region, the relationship with the Stobhill/Lady Victoria Masterplan and other local influencing factors

3 SITE ANALYSIS – a detailed review of the land use and ownership, heritage and archaeology, traffic, infrastructure and engineering, environmental, place making and architectural analysis

4 DELIVERY CONTEXT - an assessment of the property market, commercial drivers and planning context

5 COMMUNITY ENGAGEMENT – a summary of engagement activities undertaken with a range of stakeholders and the public through a variety of methods including Place Standards and identification of key themes and the emerging consensus

6 THE MASTERPLAN – an explanation of the vision, design principles and identification of the preferred masterplan solution.

7 DESIGN CODE - sets out rules and parameters for land use, scale and linkages as well as design guidance in respect of architecture, materials and urban design.

8 PHASING – setting out the preferred masterplan phasing strategy.

9 CONCLUSION – pulling together a conclusion on the opportunity at Newtongrange.

2

DEVELOPMENT
AND REGENERATION
CONTEXT

2.0 DEVELOPMENT AND REGENERATION CONTEXT

2.1 THE BORDERS RAILWAY BLUEPRINT PROGRAMME

In advance of the opening of the Borders Railway in September 2015, the 'Blueprint Group' was established (involving Scottish Enterprise, Scottish Government, Scottish Borders, Midlothian and City of Edinburgh Councils, Transport Scotland, ScotRail and VisitScotland) with the remit of ensuring the spread of its economic impact as widely as possible.

The Blueprint Group immediately recognised the importance of addressing, planning for and co-ordinating the opportunities presented by the opening of the railway and the subsequent creation of a new development corridor for developers and investors.

In November 2014 a document entitled **"Borders Railway – Maximising the Impact: A Blueprint for the Future"** was published by the Blueprint Group containing three main themes, to create:

"GREAT LOCATIONS FOR WORKING AND INVESTING"

This theme focuses on capitalising on the two-way flow created between station towns and the capital Edinburgh, connecting people to jobs and encouraging investment in business growth. For the Newton Grange masterplan, this means exploring and creating opportunity for business start-up within the town centre, in accessible locations and with higher levels of affordability than the city centre.

It also means focussing on improving the physical connections to and from the railway station itself, addressing the physical barrier of the A7/Murderdean Road to encourage best use of the station to commute to Edinburgh, the Border towns and the cross fertilisation between stations.

GREAT COMMUNITIES FOR LIVING AND LEARNING

A number of strategic housing sites are already in the pipeline along the railway corridor, encouraging a new sustainable way of living out with Edinburgh City and capitalising on the high quality of life offer within Midlothian and the Borders. This growth strategy will attract more skilled people to live in Midlothian and the Scottish Borders and in turn, increase local spend.

For Newtongrange town centre, this means ensuring that an attractive and enticing physical environment exists to capture that spend and the provision of modern, enticing facilities to encourage new residents to enjoy and support their local town centre.

For the local community, conversely the masterplan must also ensure that local people are not priced out of the market and affordability policies are in place to address this. In addition, the town centre has a role of ensuring that a 'lifecycle' of housing is achieved, leading to a more sustainable place.

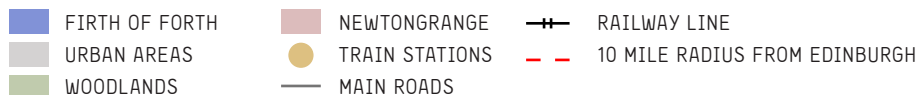
GREAT DESTINATIONS TO VISIT

It is widely recognised that tourism is key to the economic development of Edinburgh, Midlothian and the Scottish Borders. Newtongrange is home to the National Mining Museum Scotland - the finest surviving example of a Victorian colliery in Europe and a 'Recognised Collection of National Significance'. The Museum is a huge asset for the town, for Midlothian and for Scotland.

The Masterplan must ensure that it complements the work of the Museum by enhancing the village and heritage feel of the town centre, enticing people to spend the day in Newtongrange and improving connections from the town centre to the Museum itself so that there is a successful synergy between the two assets.



¹ EDINBURGH CONTEXT



2.2 EDINBURGH CONTEXT

Newtongrange is located within a 10 mile radius of the centre of Edinburgh and has a 23 minute commute time as a result of the opening of the Borders Railway. This positions the village very favourably as a desirable location for living and relocation of business opportunities. As Edinburgh's economy picks up, the village will be able to reap these benefits by providing a high quality living and working environment, both in the centre and at Stobhill/ Lady Victoria.

Edinburgh and South East Scotland is a formidable economic region, with a population of 1.3 million that contributes over £33bn of Gross Value Added (GVA) to the Scottish economy (around 30% of the country's output). In July 2017, a £1.1bn deal from the UK and Scottish Governments was secured for the 'Edinburgh and South East Scotland City Region Deal', aimed at accelerating growth of region's strengths - knowledge, culture and technology.

Funding is likely to be allocated to significant new transport infrastructure, regional skill resources and unlocking of strategic housing sites across the region. As such, growth is anticipated over the next 15 years creating 21,000 new jobs, which in turn will impact on the provision of local housing and community services.

Reflecting regional growth targets, the Midlothian Local Development Plan (MLDP) 2015 was prepared within the context of SESPlan and focused on providing for, and managing, future change across Midlothian in the form of a development strategy for the period to 2024 and a detailed policy framework to guide future land use.

The principal of new residential development in Newtongrange village centre is well aligned to the aspirations of the MLDP in terms of contributing new residential units to towards growth targets and ensuring that new housing will be located close to good community facilities, shops and employment opportunities, with efficient and high quality public transport connections.

2.3 WIDER GROWTH AND DEVELOPMENT PROPOSALS

In the years to come, Newtongrange will see significant transformation through the development of MLDP allocated housing sites. In total, 1,500 homes will be built in and around the village, resulting in a population increase of about 3,000 people – approx. 44% increase from the 2011 census figure of 5,341.

In order to accommodate this growth, a new primary school and nursery have been allocated to the east of the village at the South Mayfield site (h38), facilitated by new distributor roads to connect expansion sites to the existing village centre.

The new £36million Newbattle Community Campus is currently under construction, due for completion in 2018. The campus will provide a new library, gym, swimming pool, sports facilities (including all weather pitch) and a range of community facilities on campus.

It is also recognised that although there is sufficient GP health care capacity to accommodate house building growth in most areas of Midlothian up to 2021, beyond this point in time or if further housing sites come forward further action will be required to meet demand.



1 WIDER GROWTH AND DEVELOPMENT PROPOSALS

A BRYANS 65 UNITS (H48)
 B NORTH MAYFIELD 63 UNITS (H41)
 C SOUTH MAYFIELD 474 UNITS (H38)
 D DYKENEUK 50 UNITS (H49)
 E STOBHILL ROAD 250 UNITS (E21)
 F COCKPEN FARM 137 UNITS (H37)
 G LINGERWOOD 137 UNITS (H35)

H EAST NEWTONGRANGE 133 UNITS (H34)
 J NEWBATTLE COMMUNITY LEARNING CENTRE
 K NEWBATTLE COMMUNITY CAMPUS (2018)
 N NURSERY
 P PRIMARY SCHOOL
 S SECONDARY SCHOOL
 T REDHEUGH AND REDHEUGH WEST (H50, Hs7)



¹ WIDER COMMUNITY FACILITIES

- C COMMUNITY CENTRE
- E EMPLOYMENT
- L LOCAL CENTRE
- M MUSEUM
- S SCHOOL

2.4 WIDER COMMUNITY FACILITIES

It is clear through discussion with Midlothian Council that some of the existing community facilities within the village centre (the Newbattle Pool, leisure centre and Library will not be fit for purpose for the full duration of the MLDP period - due to either their physical condition, economical viability, or capacity to accommodate expected population increase within the village.

The preparation of Newtongrange Town Centre masterplan provides an opportunity to put in place a plan for the long term animation of the centre of the 'village' by bringing together viable uses around a community space.

2.5 STOBHILL AND LADY VICTORIA MASTERPLAN

In parallel to this commission, Midlothian Council and Borders Railway Blueprint Partnership commissioned Smith Scott Mullan to produce a masterplan for the Stobhill Depot and Lady Victoria Business Centre, Newtongrange with a view to maximise socio-economic benefits offered following the opening of the Borders Rail Line in 2015.

The emerging masterplan proposes a mixture of commercial uses such as retail and business/light industrial along Murderdean Road and adjacent to the National Mining Museum with higher density residential development in the west and central parts. Areas with more family orientated housing are proposed to be located towards the eastern end of the site.

The proposed retail will be a 'destination shopping' type which will build on the tourism offer of the National Mining Museum. This provision will be complementary to, and will not compete with, the retail offer in the Town Centre.



3

SITE ANALYSIS

3.0 SITE ANALYSIS

3.1 SITE DESCRIPTION

■ Newtowngrange is a former mining village in Midlothian, known by locals as ‘Nitten’. From its humble origins the settlement grew rapidly and by the 1890’s it was Scotland’s largest mining village, with a strong local economy, community network, sense of belonging and range of assets unlike many other comparably sized villages in that time.

The Lady Victoria Colliery, sunk by the Lothian Coal Company in 1890, came into production in 1894. It was nationalised in 1947 with the formation of the National Coal Board.

While the Lady Victoria Colliery closed in 1981 – the legacy of the mining industry persists today with the creation of the National Mining Museum (NMM) in 1984 on the colliery site, off Murderdean Road (A7). Traditional mining cottages (see image 8 overleaf) were also either sold by the Coal Board privately or to Castle Rock Edinvar Housing Association, as they remain today.

Today (see image 1 overleaf), the station and colliery (now the NMM) exist within a compact village centre – complemented by a number of shops and community facilities including Newbattle Swimming Pool, Newtowngrange Leisure Centre, a supermarket, the Newbattle Library and Newtowngrange Primary School.

The masterplan site concentrates on the village centre itself, comprising 2.63ha of land. Adjacent to the centre are the remaining colliery village cottages in a largely unaltered, distinctive linear urban pattern and forming part of the Newtowngrange Conservation Area along with the Mining Museum itself.

Despite the proximity of such assets, local geography in combination with the effects of the busy A7 corridor dividing the centre has resulted in a disjointed land use arrangement with poor connectivity and accessibility between the village centre, the train station and the National Mining Museum.

A visual analysis has been undertaken and is presented overleaf.



1 SITE DESCRIPTION

- SITE AREA
- 1 LOCATION OF IMAGES SHOWN OVERLEAF



- ¹ HIGHLY ENGINEERED RESPONSE DETRACTING FROM SENSE OF ARRIVAL
- ² MAIN ST WITH CONCENTRATION OF SERVICES AND PUBLIC TRANSPORT
- ³ VARIETY OF PUBLIC SPACES, SOME IN NEED OF A REFRESH

- ⁴ STRONG LEVEL OF ENCLOSURE TO MAIN STREET WITH HARD TREATMENT
- ⁵ LINEAR CONTINUATION OF LANDSCAPE AT THE FRONT AND REAR
- ⁶ STRONG FEELING OF GREEN AND CONNECTIONS TO WIDER LANDSCAPE

- ⁷ CHURCH BUILDING ACTS AS MAIN LANDMARK AND ADDS TO CHARACTER
- ⁸ STRONG AND SIMPLE ARCHITECTURAL LANGUAGE OF MINERS COTTAGES
- ⁹ USE OF LOCAL MATERIALS COULD BE SUPPORTED WITH 'CLEAN ROADS'

3.2 ARCHAEOLOGY AND HERITAGE

A desktop assessment of archaeological constraints and heritage assets has been carried out by ITP Energised. A summary of the identified issues within the site area has been summarised below:

- 4 Category C Listed Buildings within the Proposed Development area;
- Newtongrange Conservation Area –covers the southwestern corner of the site;
- 10 Canmore sites/ 8 Historical Environment Records (local sites identified by Historic Environment Scotland and local councils) listed within the Proposed Development area; and
- No Scheduled Monuments or historical mapping features within the Proposed Development area.

A list of existing Canmore Sites and HER sites is provided in Appendix A.

The village retains a strong mining heritage with the Miners Monument, the National Mining Museum and the cottages themselves being key elements of the history of the village. In addition the village has a history of many cultural and social movements, including the brass band events previously held in the park.

The design of the masterplan will need to take into account the above findings in producing appropriate proposals in terms of scale, massing and design in relation to the listed buildings and conservation area.





¹ HERITAGE ASSETS DESIGNATIONS

- CONSERVATION AREA
- LISTED BUILDING (CATEGORY C)

3.3 LAND USE AND OWNERSHIP

The masterplan site is mixed use (community and leisure, commercial, worship and transportation use), with multiple landowners and a range of tenants and factors. The largest land owner is Midlothian Council, who own and operate the Leisure Centre (0.31ha), Swimming Pool (0.43ha) and Library (0.30ha). Adjacent to that is the Church of Scotland, who own the Newtongrange Parish Church and Community Hall immediately adjacent (0.33ha in total). Tucked in behind Main Street is the site of the supermarket and car park (0.55ha), which is in the ownership of Brotstown Developments Ltd and long term leased to a retail chain.

To the west of Murderdean Road (A7) is the site of the train station itself, which is owned and managed by Network Rail. The station site contains 50 parking spaces, 8 of which are disabled spaces and the original station buildings, which will shortly be transformed into a cafe by Apex Scotland, providing a new facility within Newtongrange and helping to activate Murderdean Road (A7) and increase footfall between the station and the village centre.

There are a further four buildings/sites within other private ownership.





¹ LAND USE (AT GROUND FLOOR)

	VACANT		RELIGIOUS
	CAFE AND RESTAURANTS		LEISURE
	OTHER SHOPS		SERVICES
	CONVENIENCE AND FOODSTORES		RESIDENTIAL

3.4 TRAFFIC ASSESSMENT AND PARKING

Newtongrange is located approximately 12km south-east of Edinburgh city centre. The nearest town is Dalkeith, approximately 3km to the north. The village lies on the main A7, which passes close to the centre of the village, past the train station. This A-road is the main vehicular route from Edinburgh to Galashiels – approximately 40 km further south, and from there goes onto Carlisle and the M6 motorway to the South.

In 2016 the AADT flows on the A7 were in the order of 12,000 vpd through Newtongrange, of which nearly 25% were goods vehicles or buses.

The speed limit on the A7 through the village is 30mph, though anecdotal evidence suggests that this is regularly exceeded. The lack of frontage activity and straight alignment also encourages higher vehicle speeds.

The only formal pedestrian facilities crossing the A7 within the village are at the Main Street/A7 traffic signals.

All these factors combine to make the A7 a significant barrier between the railway station and the village centre. A key objective of the study from a transportation perspective will be to improve the pedestrian connectivity across the A7 between the village and the railway station.

The other principal vehicular thoroughfare in the study area is Main Street, which runs through the centre of the village from its junction with the A7 northwards towards Dalkeith. Traffic flows on Main Street are within acceptable levels for a town centre location and do not constrain development within the masterplan area.

The railway station, part of the Borders Railway, is situated on the west side of the A7 (Murderdean Road) close to the village centre. The railway station opened in 2015 and provides half-hourly services from this stop to Tweedbank and Edinburgh.

Bus stops are located on Main Street, and a number of services run through the village. These are operated by Lothian Buses and Borders Buses, as follows:

Lothian Buses

- 29 - Gorebridge - Silverknowes
- 33 - Baberton – Gorebridge
- N3 - Edinburgh – Gorebridge (Night Bus)
- R2 - Dalkeith - Gorebridge and Bonnyrigg circular
- X29 – Muirhouse – Gorebridge

Borders Buses

- X95 - Edinburgh – Carlisle
- 339 - Eskbank - Gorebridge – Bonnyrigg circular

PARKING

There are a number of car parks within the village centre and station which provide parking in close proximity to many local amenities, with the exact location and numbers shown on the diagram overleaf. Some of these are directly linked to the services which they are adjacent to whilst others are used more as “overflow” spaces by those using the station.

ACTIVE TRAVEL STRATEGY

Apart from the new connections and improvements recommended in this report, reference should be had to the Midlothian Council Active Travel Strategy to establish what other opportunities might be available to connect to the existing network of existing and proposed routes.



¹ TRANSPORT ASSESSMENT

	RAILWAY / RAILWAY STATION		CAR PARK / NO. OF SPACES
	DISTRIBUTOR ROAD		EXISTING CYCLEWAY (ATS)
	THE MAIN STREET		EXISTING FOOTPATH (ATS)
	BUS STOPS		

3.5 TOWNSCAPE ANALYSIS

Newtongrange has a distinctive village character which is strongly influenced by its mining history which is well preserved and referenced in its architecture and public realm. This unique character is recognised by the conservation area which covers the central part of Newtongrange.

The area of the village centre retains a lot of its character and represents a convergence of multiple architectural styles and character areas. Notwithstanding this, certain areas of the village centre do not contribute positively to its character or the attractiveness of the local area.

In some areas, buildings have a larger set-back and break the level of enclosure and activity of the street.

The recently opened station has a difficult relationship with the Centre and despite a strong desire line, connections across Murderdean Road remain impractical.

The character of Murderdean Road is also in need of improvement with inactive frontages created by vacant buildings and gap sites, the street remains car dominated due to the large amount of traffic on the A7.

The area around the community facilities off Main Street also has a large amount of open space provided by two large public squares. These are out of scale with the remainder of the village and are in need of renewal and reorganisation.

The church is one of the main landmarks in the area and provides a key vista from the bottom of Main Street. The village retains a strong sense of green which is formed by its rural context as well as the continuation of gardens along the main street and the historic centre.

Five distinctive character areas can be identified in and adjacent to the masterplanning site and are further discussed below.

STATION

The opening of the new station on the Borders Railway line has brought new activity to this area and with it - opportunities for change. The existing station platform is currently surrounded by a highly engineered environment and is dominated by parking. Although not unusual for a small station, there is scope for improvement and creation of a sense of arrival, through a more pedestrian focused space.

The area will be improved by the proposed cafe to be run by Apex Scotland, which will also help retain and bring back in to use a building of local character. Improvements to the way-finding information around the station would also add to the quality of the area.

The existing building further east on Murderdean Road remains vacant and detracts from the character of the area through its negative impact on to the street. An opportunity exists to create a stronger frontage to the area to give some sense of scale and enclosure to the street.





¹ TOWNSCAPE ASSESSMENT

CHARACTER AREAS

- STATION
- TRADITIONAL MINER'S COTTAGES

-  MAIN STREET
 SHOPPING
 COMMUNITY USE

- EXISTING PUBLIC REALM
- INACTIVE FRONTAGE
- POSITIVE FRONTAGE
- OPPORTUNITY FOR PUBLIC SPACE

- TRAIN STATION
- KEY BUILDING
- KEY CONNECTION
- KEY LANDMARK

COMMUNITY

This character area comprises of four key community facilities - the swimming pool, leisure centre, library and the Church of Scotland church hall. The public realm between these four buildings is mostly occupied by two public spaces. These are relatively poorly enclosed and lack the character appropriate for this village.

The surrounding buildings also have fairly negative frontages and do not respond well to Main Street or the surrounding streets and spaces.

The library and pool buildings are of a modern design with flat roof and are of no particular architectural quality. The building currently occupied by the leisure centre is of a more traditional design and used to form part of a larger building, including the now demolished social club.

The Parish Church and Church Hall are both of traditional design and are Category C listed. However the church has been closed to visitors for some time due to it being in need of major maintenance. The building itself represents a key landmark in the local area and contributes to the quality and character of the village. The redevelopment of this site would need to create a distinctive replacement for the church, to create a strong landmark.



MAIN STREET

The residential element of Main Street is quite different to some of the other character areas described in this document. It is predominantly made up of miners cottages with some more modern infill housing in between.

The dwellings are predominantly 1.5 storey high and are of a complimentary scale. Unlike the general area of cottages, the buildings are chamfered at the corner to provide a more active frontage.

All buildings are set back at least 2-3m and have front gardens which carry a sense of 'green' all the way up Main Street. Despite the more recent infill development, the rhythm and repetition of the cottages creates a strong sense of enclosure and retains elements of local vernacular within a tight and compact layout.



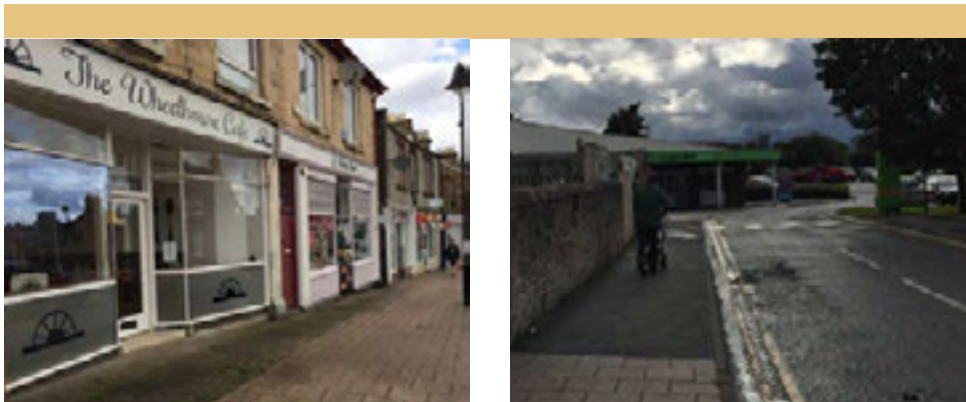
SHOPPING

The character area predominantly consists of two parts - the terrace fronting Main Street and the supermarket, sat just behind.

The terrace predominantly consists of retail, service and cafe units on the ground floor with apartments above. This animates the lower end of Main Street well and has a positive impact on the street, however it also means that most of these units back on to Murderdean Road and provide an inconsistent, inactive frontage, which is not appropriate to the scale and character of the village.

The terrace predominantly consists of traditional stone or rough cast rendered frontages with hipped or flat roofs whilst the corner of Murderdean Road is occupied by a more recent flatted development of 3 storeys in height, finished in brick and render.

The supermarket building is set back from main street and is a single storey building with a hipped roof, set within a larger parking area. Access can be had from Main Street, through the Brotstown Developments site and towards the station, which is seen as a key desire line. Currently this site does not benefit from a convenient access arrangement and has poor informal pedestrian linkages.



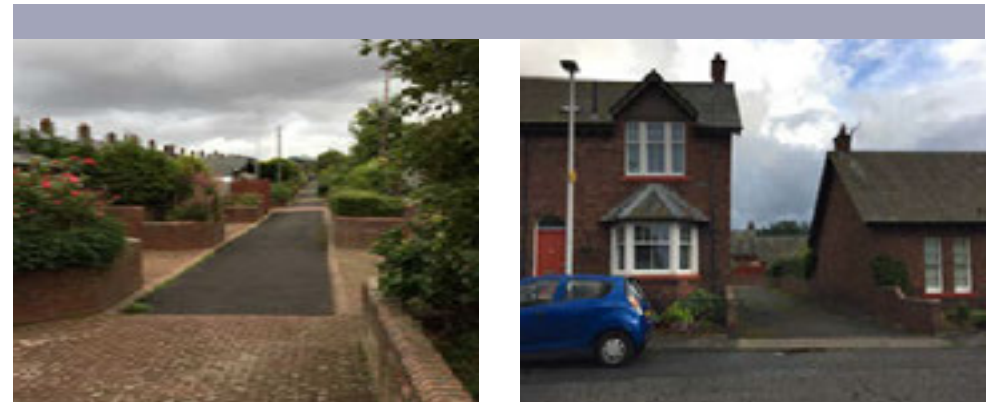
TRADITIONAL MINERS COTTAGES

The main area of the village centre is formed of traditional miners cottages and although these do not extend into the area of the site, it is important to consider their impact on the townscape of the village.

The cottages are typically 1-1.5 storeys and are of a predominantly uniform design, with modern interventions and extensions kept at a minimum. They are arranged in linear streets, running SW to NE from the Main Street. All plots are also accessed from the rear, with back lanes running parallel to the main streets. At the front, the buildings are typically set back up to 2m from the street with a front garden enclosed by a low brick and cast iron gates.

In some locations, for example adjacent to open space, larger 1.5 storey semi-detached houses can be found, which still adhere to the same uniform layout of the cottages but adding in terms of architectural language, such as through the use of bay windows and gables.

Buildings also have a very formal and positive arrangement around the existing open spaces.



3.6 STRATEGIC ENVIRONMENTAL ANALYSIS

The existing site baseline environment is not sensitive to redevelopment, particularly at a strategic level. This is not surprising given it is an urban area. Noise generated by traffic (as assessed through spot measurements) on the A7 and Main Street is the most significant aspect of the existing baseline. Traffic along these roads is a strong influence on the street level environment, especially for residents and pedestrians.

Initial noise monitoring indicates that noise, particularly from the A7, exceeds standard thresholds for amenity space. Noise levels within buildings are likely to be achievable, although for some typologies, design or site mitigation (e.g. double glazing, acoustic barriers) may be required.

In terms of ecology, the town centre has little value, although a number of the buildings have moderate bat roost potential. This would require further assessment as part of the planning process (i.e. for each phase).

For cultural heritage and archaeology, whilst there are 4 category C listed buildings and historical environment records within the site, they are unlikely to have a bearing on the masterplan design. These are aspects that can typically be addressed through the planning process (i.e. for each phase).



4

DELIVERY
CONTEXT

4.0 DELIVERY CONTEXT

A property market assessment has been carried out by Ryden and is available in the supporting documents. A brief summary is set out below.

4.1 RETAIL OFFER AND OCCUPANCY

■ Newtowngrange town centre provides a comparatively small number of local retail and retail service outlets, in two locations at the south and north ends of Main Street. The area in the south is immediately outside the masterplan area and comprises café, shops and a 'take-away'.

The 12 units here total 1,046 sq.m.; 3 of the units are vacant, indicating a vacancy rate of 25%. Floorspace vacancy is higher at 34% due the larger former RBS and Post Office outlets.

In 2016 an overall 5% retail vacancy rate was recorded for Newtowngrange, lower than the 6.7% vacancy rate recorded for Midlothian as a whole . Although 3 units noted above are vacant, none is being marketed and in fact none is within the town centre masterplan area.



1 EXISTING BUILDINGS

- | | |
|--|--|
| 1. NEWGRANGE PARISH CHURCH & HALL 964 SQM. | 5. APARTMENT OVER RETAIL 161 SQM. |
| 2. LIBRARY 267 SQM. | 6. SUPERMARKET 1323 SQM. |
| 3. NEWBATTLE POOL 938 SQM. | 7. TRACK 2 TRAIN (CHARITY CAFE) 188 SQM. |
| 4. LEISURE CENTRE 590 SQM. | 8. OLD PAY OFFICE (VACANT BUILDING) 136 SQM. |

Strategic store closures have affected Newtongrange town centre in the last 5 years, including Royal Bank of Scotland, Allan Smith Bakers and the Post Office. To counteract the closure of the bank a mobile banking service by the RBS visits the town twice a week. There is also an active franchise opportunity by the Post Office for services in the town. There have however been store openings in the last few years including The Barber Shop (in former Allan Smith Bakers), Daisy Chain florists (in former C Ovens unit) and Blakes Barbers (former Candy Cupcake). Although the RBS unit has been sold it remains unoccupied.

Supermarkets in Newtongrange are represented by a store behind Main Street (in the town centre) with smaller convenience store Newton Convenience Store also in the town centre and elsewhere in the town a Scotmid and a Keystore. The store has a sales area of 1,206 sqm and is understood to be held on a long lease to Brotstown Developments (who were consulted by the masterplan team).

A full copy of the retail assessment, prepared by Ryden, is available in the supporting documents.

4.2 EMPLOYMENT AND RESIDENTIAL MARKET ASSESSMENT

From an employment property market perspective, weak economic growth over recent years in Scotland (excepting Q1 2017) is a concern. The property market assessment carried out by Ryden assesses the strength of the office, industrial and residential market in Newton Grange. The office market in Newtongrange is very limited. Offices are located within retail frontages in the town centre, or at Lady Victoria Colliery (to the south of the town centre adjoining the A7 trunk road). Occupiers in the town centre are Abacus Accounting, Lang Syne Publishing and Swift Taxis.

There is no industry in the town centre. More widely in Newton Grange industrial sector occupancy rates are very high and new development is limited.

Registers of Scotland (RoS) reports that the average price of a residential property in Midlothian in April 2017 was £159,575, an annual rise of 2.2% which is below the national average of 6.8%. ESPC records an average house price of £188,516 in Midlothian for 3-months March – May 2017, with an annual rise of 6.5% which is higher than the RoS figure.

According to the Scottish Government there were a total of 947 new build starts in Midlothian during 2016, of these 847 were in the private sector, with 100 in the social sector. This means that Midlothian accounted for 5.6% of Scotland's housebuilding in 2016 with 1.6% of the country's resident population.

There are two large-scale housing developments underway in Newtongrange at present, with an additional development planned. None of these developments are in the town centre. However, the Town centre masterplan offers an opportunity to capitalise on this growth creating an environment that will attract new residents to the centre.

4.3 DEMOGRAPHIC ANALYSIS

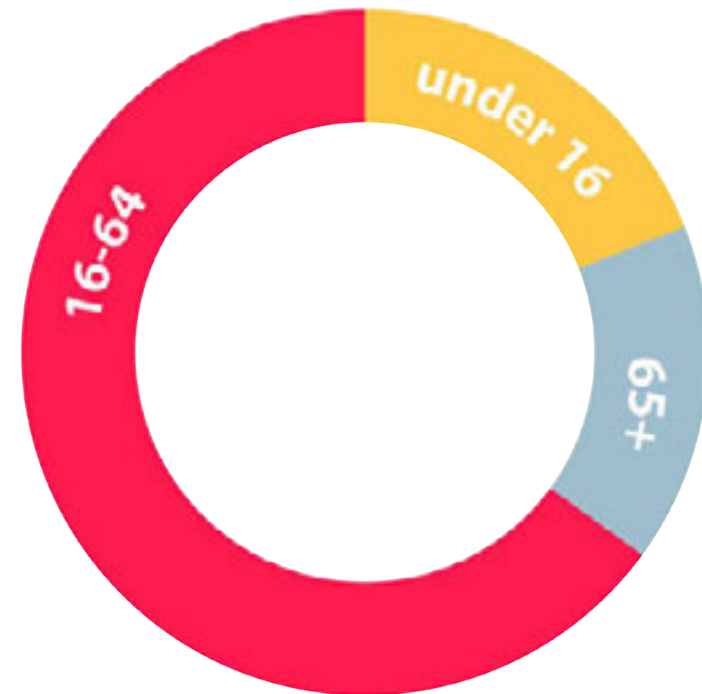
Newtongrange is a former mining town located in central Midlothian with a resident population 5,341, which is 6.4% of the local authority area's population. At the 2011 Census there were 2,252 households in Newtongrange, also equivalent to 6.4% of the Midlothian total. The opening of the Borders Railway in 2015 has given the town improved transport links north to Edinburgh and south to Tweedbank (Galashiels).

'Midlothian moving Forward, Community Planning for Midlothian - Newtongrange Neighbourhood Profile July 2016 points out that "according to the 2011 Scotland Census, there were 690 people aged 65 or older living in Newtongrange. This is 5.0% of Midlothian's 13,903 people aged 65 or over. By 2037 there is predicted to be 24,781 people in Midlothian aged 65 or above. Assuming there is little change in the demographics of Midlothian and Newtongrange, there will be 1,230 people over the age of 65 in Newtongrange by 2037.

Given this likely increase in those over the age of 65 the masterplan will take the need for more sheltered housing into consideration when considering sizes and tenures of proposed new properties.

The Newtongrange Community Action Plan 2017 – 2022 notes that "Midlothian has particularly high demands for affordable housing due to its proximity to the capital. Although the council has invested heavily in new build social housing in recent years, the council's waiting list for affordable housing continues to increase".

Within Newtongrange Midlothian Council own some 267 units of affordable housing.



NEWTONGRANGE POPULATION SPREAD BY AGE GROUP
(FROM NATIONAL RECORDS OF SCOTLAND JUNE 2014 ESTIMATES)

4.4 AVAILABLE FUNDING/COMMERCIAL DRIVERS

Based on the above assessments of the village demographic, property and retail markets, the following development drivers can be identified.

The redevelopment of the village centre will be predominantly housing led. This is due to the strong demand in the area for new private housing and assisted living premises.

There will be opportunities for retail and service units to be developed in the town centre however this will mostly be restricted to smaller units (around 100sqm) and a single larger unit, which would compliment the existing provision. Larger retail space will be based on any potential demand from existing providers in Newtongrange.

Opportunities also exist to rationalise and bring under one roof existing public services with the village centre, which could bring forward other opportunities in the site and elsewhere within the village to utilise vacant land.

Animating the town centre and increasing foot-fall and presence throughout the day will create scope for an uplift in quality and value of shops and businesses, especially as the impacts of the Border Railway consolidate.

4.5 PLANNING POLICY CONTEXT

Midlothian is recognised within Scotland's Third National Planning Framework (NPF3) (2014) as one of Edinburgh and the south east city regions. Town Centre Planning Policy is underpinned by the Scottish Government's Town Centre First Policy, which is recognised in both Scottish Planning Policy (SPP, 2014) and NPF3.

SESPlan (2013) is the Strategic Development Plan (SDP) for the area, which sets out the spatial strategy for the six member authorities of the south east: City of Edinburgh, East Lothian, Fife, Midlothian, Scottish Borders and West Lothian Councils. Policy 3 states that, Local Development Plans will:

"Identify town centres and commercial centres clearly defining their roles; Support and promote the network of centres as shown in Table 1, and identify measures necessary to protect these centres including setting out the criteria to be addressed when assessing development proposals; and Promote a sequential approach to the selection of locations for retail and commercial leisure proposals. Any exceptions identified through Local Development Plans should be fully justified."

Newtongrange is identified as one of the North Midlothian towns located along the A7/A68 / Borders Rail Corridor as an established, attractive and accessible town for development.

The Midlothian Local Development Plan (MLDP) was adopted on 7th November 2017. It provides the development strategy and policy framework for the next 10 years and is the basis for determining planning applications in Midlothian.

The MLDP identifies a series of town centres in Midlothian; Bonnyrigg, Dalkeith, Gorebridge, Loanhead, Mayfield, Newtongrange, Penicuik and Shawfair, as required by SESPlan.

Newtongrange is identified as an 'Other Town Centre' whose role is to primarily serve the needs of those in Midlothian for shopping, commercial leisure and office facilities. Growth and diversification is supported through policy TCR1 'Town Centres':



¹ AREA OF LOCAL PLAN POLICY SHOP 1

- Proposals for retail, commercial leisure development or other uses which will attract significant numbers of people will be supported in Midlothian's town centres, provided the scale and function is consistent with the town centre's role, as set out in the network of centres
- Change of use from retail will only be permitted if the subsequent use is one which contributes positively to footfall in, and the vitality of the town centre;
- Conversion of ground floor retail uses to residential will not be permitted however conversion of upper floors to residential is supported
- Proposals for open air markets will be supported
- Newtongrange Conservation Area is located adjacent to the town centre and therefore, any potential development within the masterplan area should take the conservation area into account in terms of appropriate character, scale and appearance.

5

COMMUNITY
CONSULTATION

5.0 COMMUNITY CONSULTATION

5.1 COMMUNITY CONSULTATION EVENTS

As part of our commission, *LDA Design* undertook a series of individual stakeholder engagement sessions throughout August and September 2017. 'One to One' meetings began with a discussion to inform stakeholders of *LDA Design's* appointment and our brief for the regeneration of Newtongrange Town Centre.

LDA Design and the wider consultation team met with individuals from the following organisations in 1-1 meetings (minutes to meetings can be found in the accompanying Engagement Summary Report):

- Newtongrange Community Council
- Smith Scott Mullen
- Colliers
- Brotstown Developments
- Church of Scotland
- Newtongrange Community Learning Centre
- Newtongrange Community Council
- Midlothian Council Community Services
- Network Rail
- Scotrail
- Local Store operator
- Apex Scotland
- Castle Rock Edinvar Housing Association

Individuals from the below organisations attended the stakeholder consultation :

- Newtongrange Community Council
- Newtongrange Community First
- Orchard Grange
- Newbattle Community Centre
- NHS Lothian
- Newbattle Community Nursery

- Newbattle Leisure Centre
- Midlothian CPR
- 21st Midlothian Scout Group
- Midlothian Council Economic Development + Community Services
- Business Gateway

STAKEHOLDER CONSULTATION OVERVIEW

The Stakeholder Workshop Event was held at The Newtongrange Mining Museum on Friday 1 September 2016. A series of group sessions were held throughout the day to gather a feeling of perceived problems and solutions to these within the village centre at present. This was followed by a drawing exercise to allow the Stakeholders to see potential spatial resolutions to these resolutions. The Stakeholders were also given the chance to complete questionnaires and place standards tools compiled in section 1.4.

COMMUNITY CONSULTATION OVERVIEW

In follow up to the earlier Stakeholder Sessions, *LDA Design* held Community Workshops on 2 September 2017. For this the community was invited by maildrop, through Facebook and a webpage was promoted through the Midlothian Council website.

The Community Engagement day was held in Newtongrange Parish Church on Saturday 2 September. It followed a similar format of the Stakeholder event. Beginning with an overview of project, encouraging thinking of issues/opportunities in the town through a post-it session. This was then followed by a more in-depth hands on planning session. The process was repeated in both am/pm sessions and a drop-in facility was available as a catch-all. Questionnaires were given to all attendees.

OUTCOMES

The session recorded the discussions on flipboards and post-it notes and ideas following on from these were drawn in to several plan options with attendees. A full record of the outcomes is available in the Stakeholder Consultation Summary.



5.2 KEY THEMES

A summary of the main commentary received through LDA Design's stakeholder and community consultations undertaken in August and September 2017 has been summarised under the main headings of **Problems / Issues**, **Dreams** and **Solutions** and then subdivided under the headings:

- Community Facilities and Activities
- Public Space and Environment
- Heritage Assets
- Transport, Parking and Servicing
- Active Travel and Green Networks

During consultation the strong feeling that the character of Newtongrange is that of a village (rather than a town) was communicated, as well as a desire to retain services and facilities within the village.

5.3 DRAFT MASTERPLAN CONSULTATION

A final consultation event was held on the 27th of November at the National Mining Museum. The event was an opportunity for local residents to comment on the emerging themes and spatial proposals.

The event was well attended with around 250 attendees throughout the day. The full summary of comments, recommendations and concerns is available in the Engagement Summary, which supplements this report.





6

MASTERPLAN

6.0 MASTERPLAN

6.1 DEFINING 'SUCCESS'

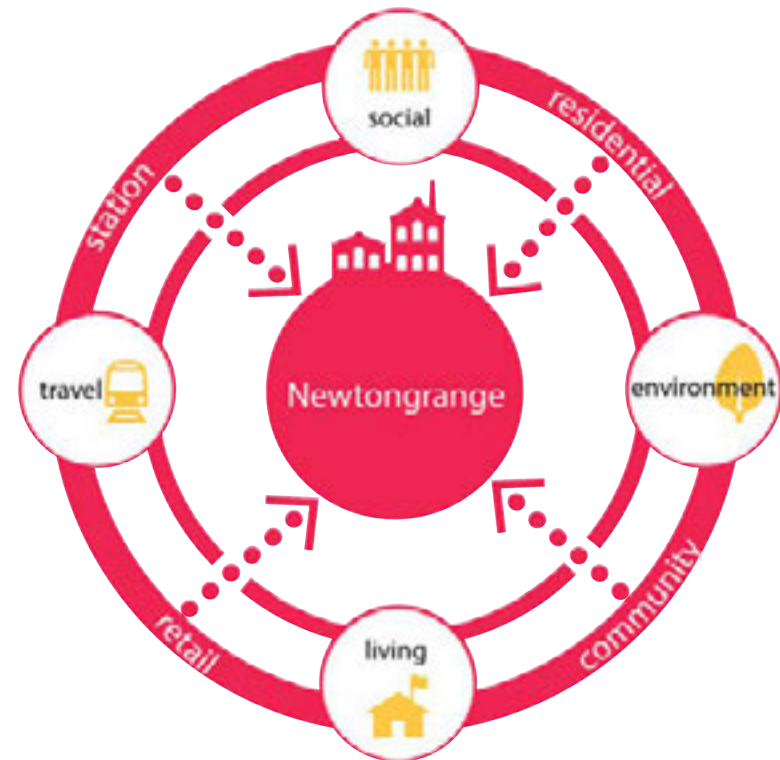
■ The village possesses some strong placemaking qualities which reinforce its distinctive character and relate to its history, geography and local identity. Despite the recent and future growth of the residential areas within Newtongrange, its centre maintains a village character due to the scale of the buildings and streets. Larger-scale town centre development would not fit with the underlying urban grain and would be detrimental to character. Further, such development would not be in line with the way people shop in the local area.

The key for the masterplan is therefore to reinforce the main existing qualities of the village, which are:

- Important heritage of the mining past;
- A wide mix of community uses;
- Good quality and well preserved traditional architecture within the areas of miners cottages;
- A strong community with a vision for the village; and
- A Borders Rail station creating a convenient connection to Edinburgh.

Retaining the historic character and providing key connections to the station will put the village in a strong position to benefit from the Borders Railway programme in the future.

The likely closure of facilities as a result of the opening of the Community Campus and the lack of retail demand in the village centre poses a fundamental challenge for the masterplan. The only viable solution to this is to concentrate the community uses in the centre, which will help to increase footfall and animation of the centre and attract more users and services to use the village on a regular basis. However, community uses generally do not create economic value and



a strategy of animating the centre with such uses brings a significant delivery challenge.

This delivery challenge will be offset by housing development to create value which will need to reflect in particular the needs of elderly local residents to ensure opportunities for down-sizing are available for those who need them.

6.2 VISION AND DESIGN PRINCIPLES

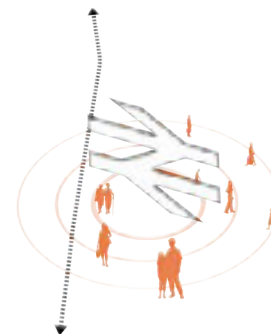
VISION

Newtongrange will have a reinvigorated new “heart” where the community will be able to access services, shopping and leisure opportunities all based around a new square which will become a place of meeting, markets and activity. Set around the square, will be new high quality housing of different types and tenures, providing homes at the heart of the village.

DEVELOPMENT PRINCIPLES

5 key principles have been developed to help deliver this vision:

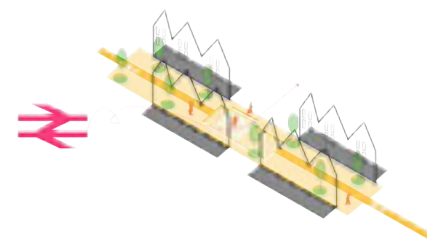
- Create a new “village hub” by integrating viable community uses under one roof, set around a village scale public space at the “heart” of the Main Street;
- Connect the village heart to the train station and National Mining Museum through a safe and attractive link;
- Fill existing gaps within the village centre with high quality contemporary homes which would be respectful to the village scale and character;
- Create a sense of arrival at the station to reinforce the village character and attract new visitors; and
- Change the character of Murderdean Road to a village street, including a safe crossing to the station.



CREATE A NEW ARRIVAL SPACE AT STATION



FILL EXISTING GAPS WITH NEW HOMES



CHANGE THE CHARACTER OF MURDERDEAN ROAD



CREATE A VILLAGE SCALE PUBLIC SPACE



CONNECT THE “VILLAGE HEART”, STATION AND MUSEUM

6.3 PLACEMAKING FRAMEWORK MASTERPLAN

The regeneration of the village centre will establish a new key space which will create strong connections to existing services and enhance this further with new retail opportunities and attractive contemporary dwellings which will respect the historic character of Newtongrange.

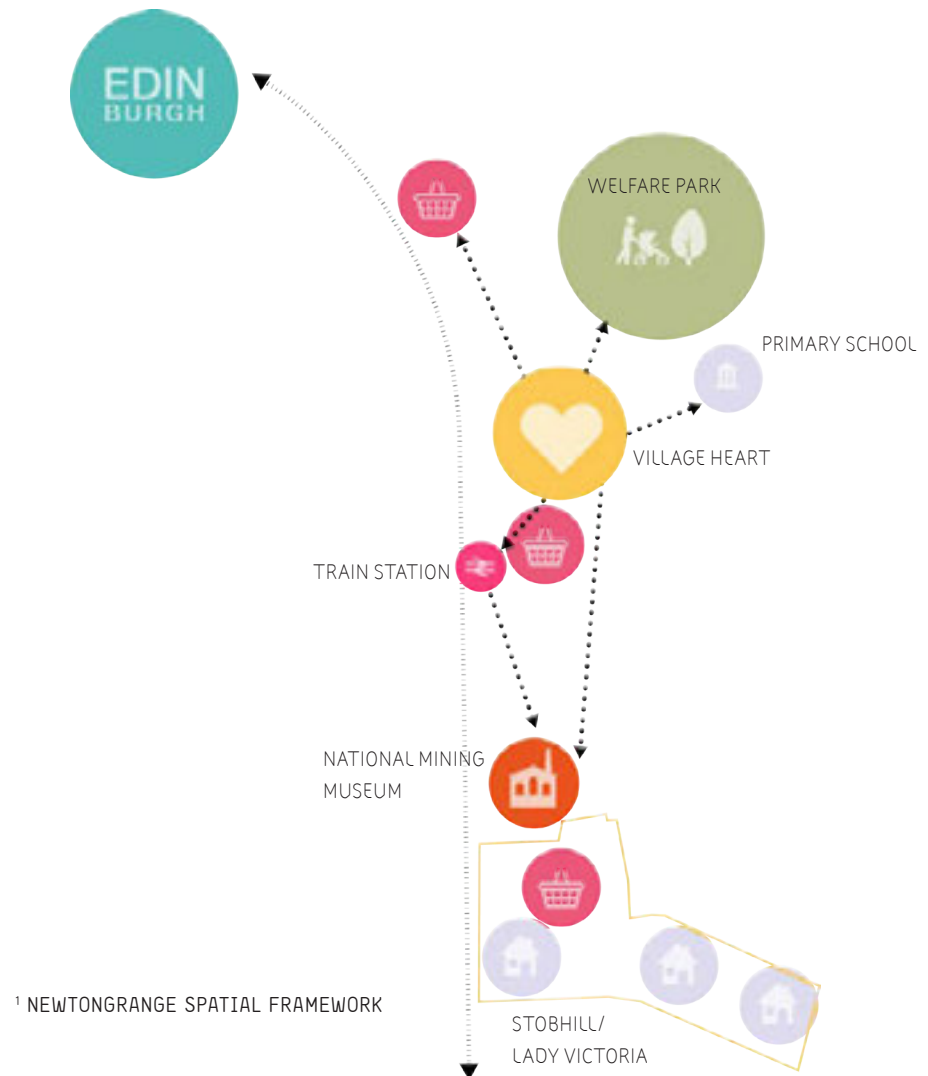
The new centre will be well connected to the Museum and Lady Victoria site which will provide a complimentary retail and employment offer as well as new residential dwellings of different sizes and tenure.

The village centre will be well placed to become a destination, and will be an attractive proposition for people who work in the town, those wanting to commute to Edinburgh and those coming to visit Newtongrange.

New connections and spaces are shown in a conceptual diagram on the right.






The Site Framework Diagram overleaf shows the placemaking objectives, to be achieved within the masterplanning site. The new centre should achieve new active frontages along key routes and re-establish character on Murderdean Road. Improvements to public realm and a new attractive connection between the station and Main Street will become the spine for new residential development.

These key masterplan moves need to retain the green character of the village and respect the scale and architectural style of the historic centre whilst reinstating key frontages and creating new places for people.





¹ SITE FRAMEWORK DIAGRAM

	SENSITIVE FRONTAGES		OPPORTUNITY FOR PUBLIC SPACE		TRAIN STATION		OPPORTUNITY FOR IMPROVED CROSSING
	AREAS OF PUBLIC REALM IMPROVEMENT		KEY PEDESTRIAN GATEWAY		KEY CONNECTION TO TRAIN STATION AND NATIONAL MINING MUSEUM		KEY LANDMARK
	AREA IN NEED OF CONSISTENT TREATMENT		OPPORTUNITY FOR IMPROVED FRONTAGE		SENSE OF "GREENNESS"		IMPORTANT VIEW
	BUILDING TO BE BROUGHT INTO USE		KEY RETAIL FRONTAGE				

6.4 OPTIONS ASSESSMENT AND PREFERRED OPTION

Throughout the design process the site was broken down into sub-areas which were considered on their merit and also on their contribution to the wider site. Options for each area were developed based on responses to issues which emerged through public engagement. A more detailed review of each option considered, is included in Appendix B of this report.

The development and assessment of the masterplan included inputs on the viability of certain design decisions, impact on traffic, retail capacity, compliance with planning policy as well as review of its placemaking potential. Viability emerged quickly as the key challenge to the development and delivery of the masterplan. In simple terms all options considered struggled to create sufficient value to offset costs. This was due primarily to the low residential values in Newtongrange combined with the lack of financial return from investment in community facilities. Viability is also strongly dependant on the delivery route. For example a public sector led scheme with low finance costs and non-commercial profit expectations would be more viable than the private sector version of the same scheme. Further, involvement by a community capable of raising funds and delivering projects can further enhance viability.

To test deliverability two schemes were therefore appraised on a commercial basis. An ‘aspirational’ option tested a large multi-use Community Hub suitable to replace, for example, the existing facilities at Gardiner Place. A ‘Pragmatic’ option sought to maximise housing to create value and aimed to minimise the costs related to the provision of non-value generating elements. Both options are achievable within the parameters set out in this document. The illustrative masterplan opposite shows the ‘pragmatic’ option in which costs are minimised and value is created by maximising housing.

The option includes the redevelopment of the church site for residential use and a new Church Centre which would provide worship space as well as an area for events and activities.

INDICATIVE DEVELOPMENT SCHEDULE

UNIT	NUMBER IN DEVELOPMENT	FLOOR AREA (SQM)
1 bedroom flat	44	50
2 bedroom flat	58	57
2 bedroom house	16	78
3 bedroom house	40	96
Church Centre	1	308
Community Hub	1	200
Large retail unit	1	700
Small retail units	1	100
	4	63
Office development	1	1000

The “community” site would create a new public space just outside a community hub building. This area would also be supported by high quality residential dwellings of various size and tenure. The square would be enclosed by retail units on either side of Main Street.

The Brotstown Developments site would be redeveloped to deliver a mixture of apartments and family homes and would create a safe and attractive route from the train station to the centre of Newtongrange. This will be a key route which will improve the integration of the town centre with the station and Museum. The station site will be improved through the opening of a cafe by Apex Scotland and could further be supported by improvements to the public realm. The old Pay House on Murderdean Road would be redeveloped into an office building with associated parking area.

The illustrative masterplan option corresponds to the spatial framework principles set out previously and achieves a high quality public realm in the form of a new public square as well as a community building and new retail opportunities, supported by a mixture of residential dwellings. An indicative development schedule for the illustrative masterplan is shown above.



¹ ILLUSTRATIVE* MASTERPLAN
(SCALE 1:2,000 @A4)

- PROPOSED DEVELOPMENT
- PRIVATE AMENITY SPACE
- PUBLIC OPEN SPACE
- SHARED SPACE

- NEW VILLAGE SQUARE
- SERVICING AREA
- PAVEMENT AND FOOTPATHS
- PROPOSED TREES

² GROUND
FLOOR USES

- SMALL RETAIL
- LARGE RETAIL
- FLEXIBLE RESIDENTIAL/
RETAIL UNITS
- OFFICE
- COMMUNITY HUB

6.5 A NEW COMMUNITY HUB

The centre of Newtongrange faces a significant challenge in respect of its public facilities. When the Newbattle Community Campus opens, it is anticipated, that the demand for some of the facilities in the village centre will reduce and others will be closed due to duplication of uses and financial pressures.

In other regeneration schemes the reduction in footfall as a result of the closure of community facilities might be offset by an increase in commercial or business development. In Newtongrange the demand for such uses is limited and the village centre therefore risks a spiral of low footfall and continued business closures. In the foreseeable future the key means to reverse and confront this risk is by concentrating viable community uses at the heart of the village to create animation and footfall to support businesses. This has the potential to create a virtuous cycle in which increased footfall drives business success which in turn leads to further footfall.

A crucial part of this plan therefore is the creation of a purpose built community hub to house viable community services and commercially run community assets such as meeting rooms. The other key element of the plan is to support the Church in the development of their land to bring forward appropriate housing to meet local need and a new Church Centre.

As noted previously however this strategy is accompanied with a significant viability challenge due to the economic values associated with community uses. This viability challenge will need to be addressed through the delivery process.

The new Community Hub could be designed in a variety of ways, depending on the needs of the community and whether the Church has facilities within the hub. The design can also be flexible to allow the Church to provide facilities as part of the hub.

6.6 NEW VILLAGE SQUARE

Squares play an important role in the community of a village. The diagrams below give an idea of the scale and size of space of a range of squares in existing towns and villages.

These existing squares typically accommodate a range of activities, vehicular and pedestrian movement through the square, provide some parking and often some landscaping. There is often a concentration of commercial and community uses in buildings that front onto the square. Squares also act in orientation and navigation through the village as a whole and aid in giving a place its distinctive identity.

These precedents have informed the ideas for a new square for Newtongrange and highlight some of the key issues the design of a square needs to address.

The key priorities for Newtongrange Square are that it is a pleasant and attractive place for the community that accommodates people well – for example by providing outdoor seating and space for potential events such as music or markets. It should be easy to navigate for pedestrians and sensitively accommodate parking for the community, retail and residential uses adjacent to it in such a way that pedestrian uses form the dominant function. The space should be overlooked by buildings, with active frontages that also enclose and define the space.

To function well the space will have a flexible south facing space outside of the Community Hub large enough to accommodate a small market and providing an opportunity for external seating should either the hub or the adjacent uses include a café; and, a parking space to the north of the proposed retail unit.

The design of the space should allow for larger one-off events, perhaps an annual fete or musical event, by combing the two zones described above and suspending parking. This flexibility will be key to the success of the space as a focus to the community.

The space occupies a prominent location in Newtongrange and will be contained by three of the most important civic and retail buildings; the Community Hub, the Church Centre and the anchor retail unit. The design adopted and the quality of materials used will therefore be of paramount importance to ensure that the space properly functions as a real civic and symbolic heart to the community.





¹ ARTISTS IMPRESSION SHOWING NEW VILLAGE SQUARE, COMMUNITY HUB AND RESIDENTIAL UNITS AT THE BACK



6.7 TRAFFIC ASSESSMENT

The traffic generation for the existing and proposed land use have been compared to assess whether there is likely to be a net increase or decrease in overall traffic levels. The predicted daily trip rates associated with the various land uses have been taken from the TRICS database.

The analysis indicates that the Aspirational option is predicted to generate significantly fewer vehicle trips than the existing land uses that it replaces. The biggest single generator is the supermarket and the fact that this is virtually halving in size in the preferred option is a major contributor to the net reduction. The local road network should therefore benefit from lower overall traffic flows.

PARKING

Parking is being provided throughout the masterplan area as follows:

Supermarket site:	42 spaces
Church site:	43 spaces
Community Site:	68 spaces
Station (office):	10 spaces
TOTAL:	163 spaces

Thus the masterplan proposals will not lead to a reduction in overall parking. The residential parking requirements have been assessed, based upon the following standards:

Housing:	1 space per house:
Flats:	1 space per 2 flats
Visitor:	1 space per 5 dwellings

These reduced standards are considered acceptable due to the town centre location, proximity of the site to the station and the likely amount of affordable and sheltered dwellings in the housing mix.

PREDICTED TRIPS - EXISTING LAND USES

USE	SIZE (SQM)	DAILY TRIP RATE*	DAILY TRIP
Parish Church and Hall	964	5.665	55
Library	267	16.309	44
Newbattle Pool	938	21.305	200
Leisure Centre	590	21.305	126
Apartments (over retail)	4 (No.)	1.730	7
Retail	161	95.166	153
Supermarket	1321	99.349	1314
Track 2 Train (cafe)	188	27.714	52
Old Pay Office (vacant)	1136	-	-
TOTAL			1,950

PREDICTED TRIPS - PREFERRED OPTION

USE	SIZE (SQM)	DAILY TRIP RATE*	DAILY TRIP
New Church Centre	308	5.665	17
Community Hub	200	10.473	21
Retail Unit	800	99.349	795
Offices	1000	3.503	35
Apartments	102 (No.)	1.730	176
Houses	56 (No.)	3.630	203
TOTAL			1,147

* Non-residential trip rates are per 100sqm GFA

6.8 ENVIRONMENT IMPACT ASSESSMENT OF ILLUSTRATIVE OPTION

As the existing environment is not particularly sensitive to re-development, the impact of differing design options is negligible.

Traffic noise is the most influential environmental aspect, especially from the A7. This is an important consideration for Phase 4 of the Preferred Option. Buildings are proposed with frontage onto the A7, which is currently on a slightly higher elevation. Noise is likely to impact front bedrooms and may be avoided by locating bedrooms at the rear. Amenity space (gardens), may also be impacted, especially as some gardens have a direct line of sight with the A7. Further noise modelling and assessment will be required as part of any future planning application. This may necessitate inclusion of mitigation measures (e.g. acoustic barrier) to achieve desirable noise levels.

It will be important to create suitable open amenity space and landscape areas with native plant species to enhance biodiversity. This should be applied throughout each phase. Any measures to enhance urban biodiversity should be undertaken involving community groups and in the context of the objectives of the Midlothian Local Biodiversity Management Plan.



7

DESIGN CODE

7.0 DESIGN CODE

7.1 DESIGN CODE PLANS

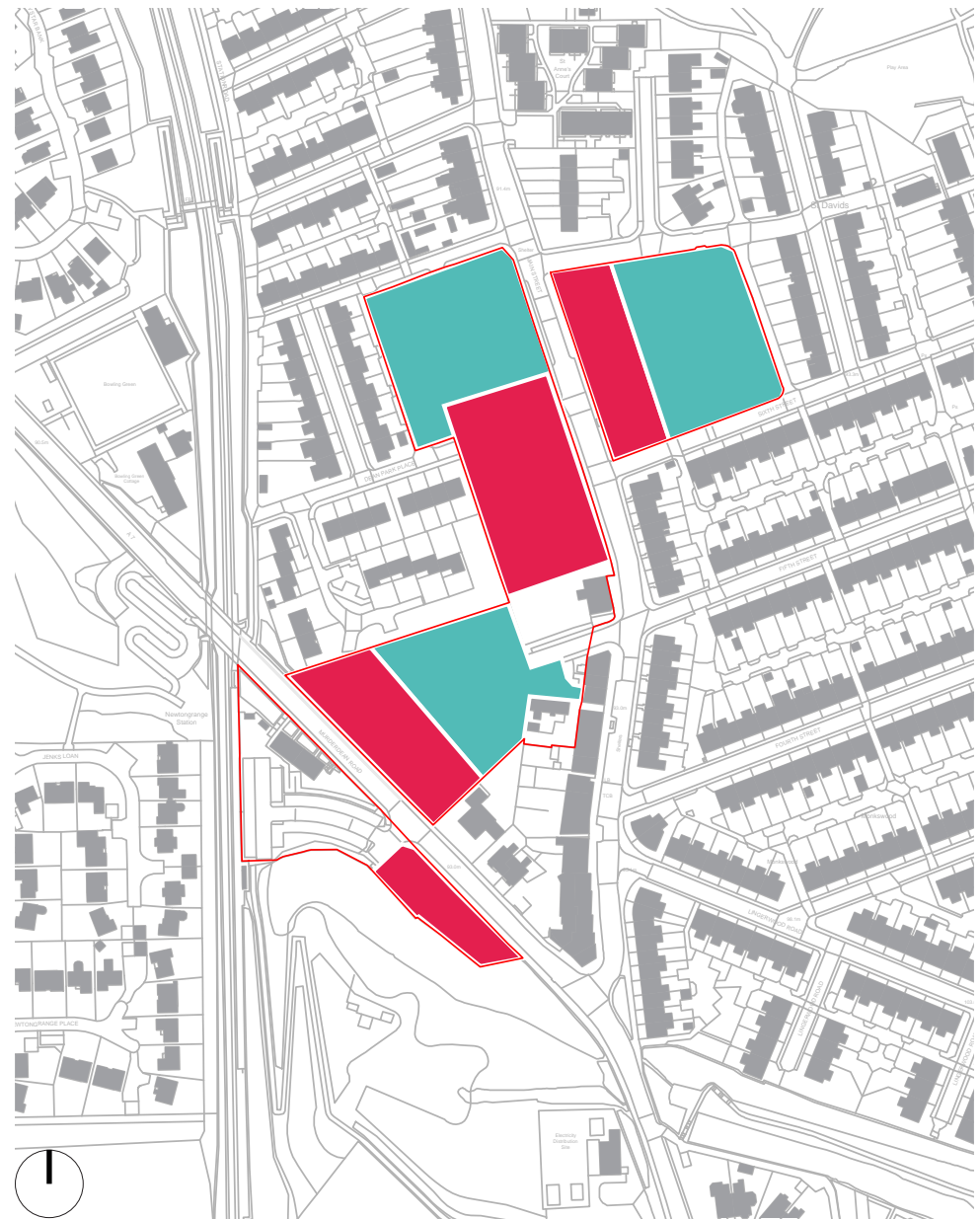
The following section presents the design code plans relating to the scale, use and movement within the masterplan site. The plans carry through the key qualities of the vision, design principles and spatial character and are flexible enough to allow a variety of solutions to the design of the site.

SCALE

The Code Plan for scale shows the distribution of building heights around the site. This is predominantly based on the townscape of the existing centre and relates to its scale and character.

The larger area of the site would be developed to a maximum of a 2 storey height to relate to the more intimate historic scale of the centre.

At certain parts of the site, such as along Murderdean Road and at the new village square, 3 storey buildings can be appropriate to create a better enclosure to the surrounding street.



1 SCALE CODE PLAN

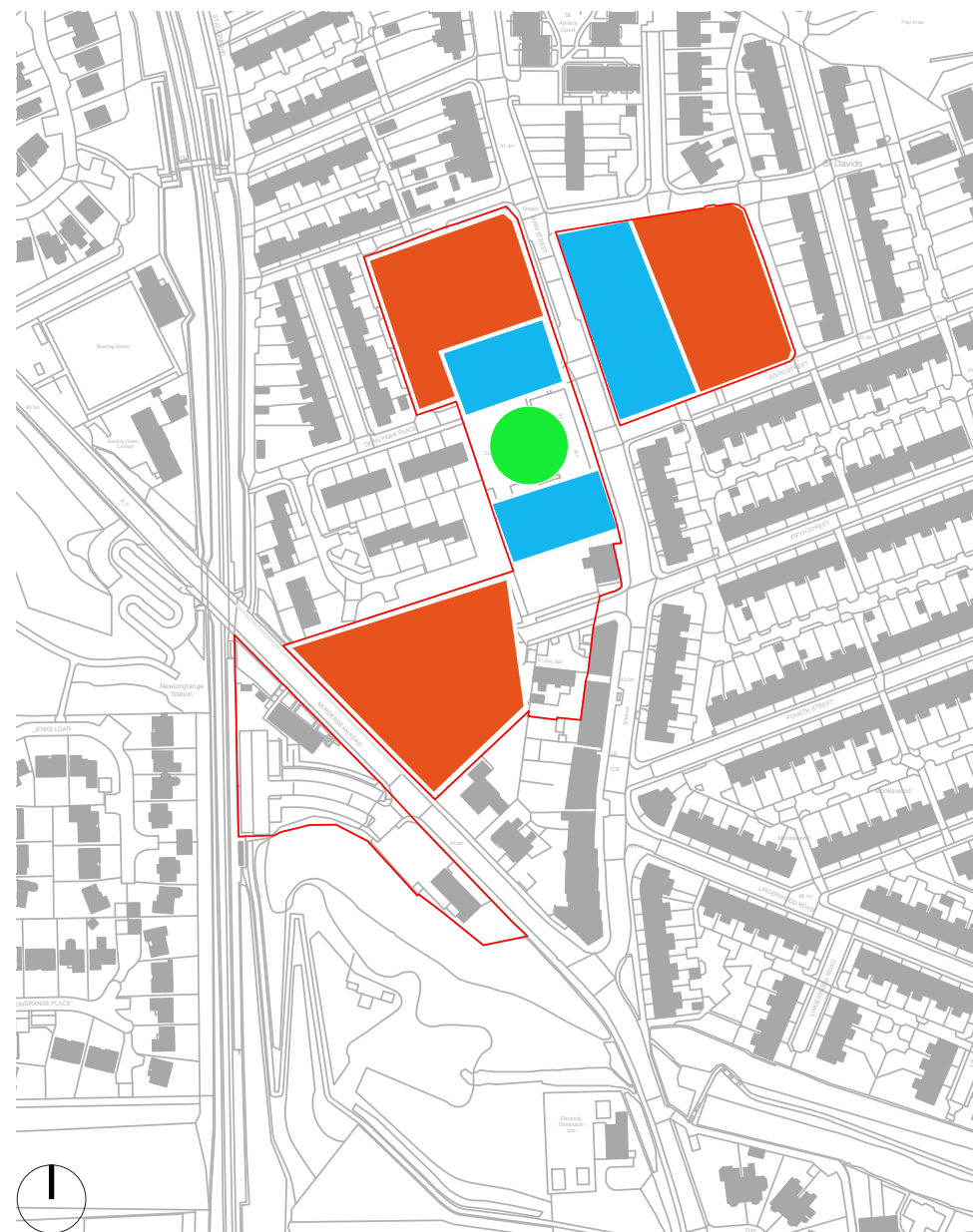


LAND USE

The Code Plan for Land Use shows the distribution of development within the site as well as opportunities for new public space. It is proposed to develop most of the available land to make efficient use of the land in the centre of the village.

Apart from the main public space, other incidental open spaces can be created within the residential areas. In the design of the main public space, principles set out in Section 6.6 should be used as a guide to designing a pedestrian friendly multi- functional space.

Where smaller retail units are appropriate, the ground floor of the unit should be used as a shop or alternatively as an office, workshop or café with living accommodation upstairs.



¹ LAND USE CODE PLAN

- KEY PUBLIC SPACE
- ACTIVE GROUND FLOORS (RETAIL/HUB/CHURCH CENTRE WITH RESIDENTIAL ABOVE)
- RESIDENTIAL

MOVEMENT

The Movement Code Plan shows the key pedestrian and vehicular routes and their relationship to the proposed areas of development and the train station. The main pedestrian connection would provide an improved link between the village centre and the train station and Museum, aided by a new crossing on Murderdean Road. Another pedestrian access route could be created through the church site to allow for easier access to the GP, school and Wellbeing park.

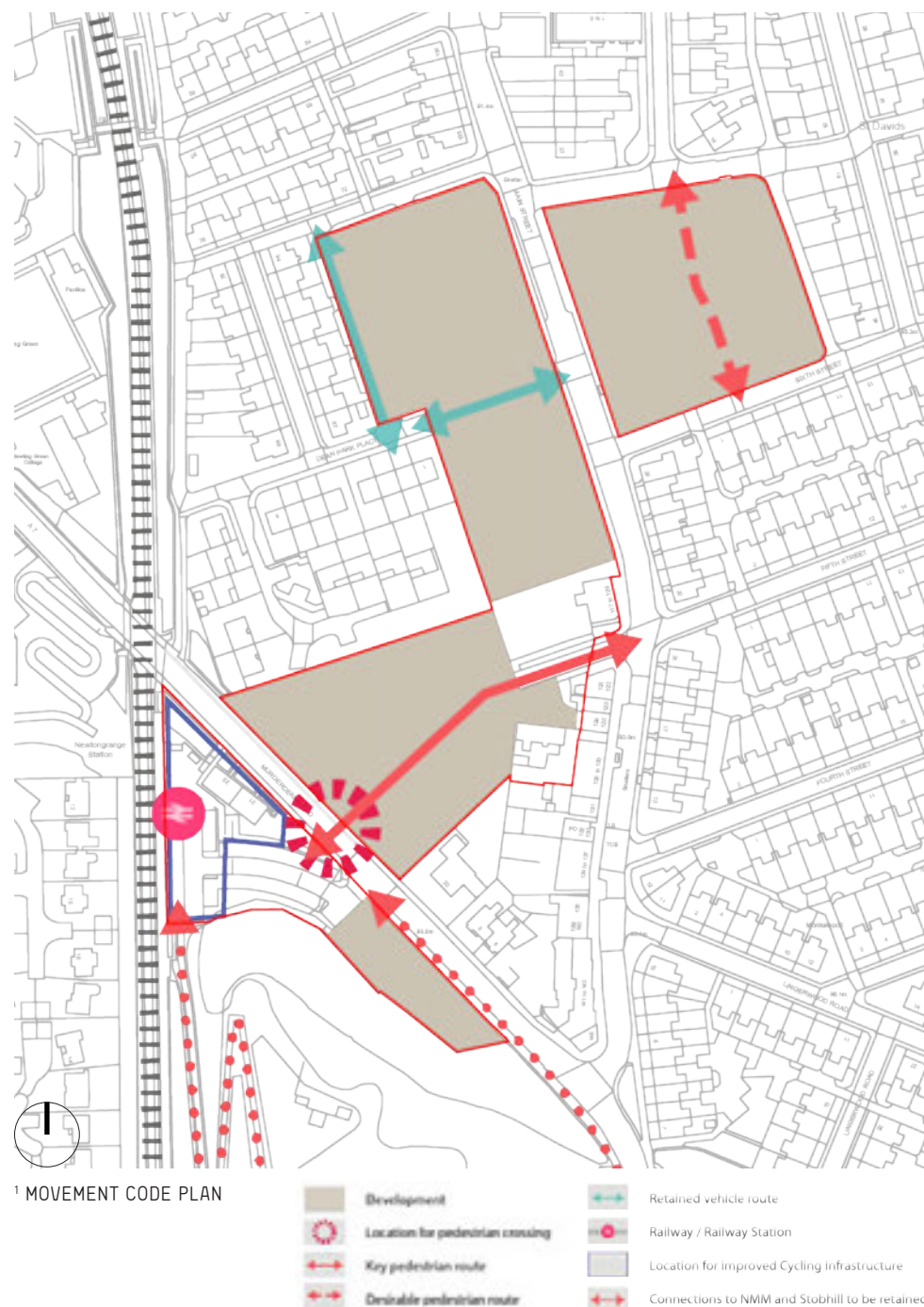
Vehicular connections need to be maintained between Main Street and Dean Park Place and along the back lane connecting Dean Park Place and Dean Park. Other pedestrian and vehicular routes can be established within the masterplan area as appropriate.

A7 PEDESTRIAN CROSSING

An important objective of the study is to improve the pedestrian connectivity between the rail station and the town centre, separated as it is by the main A7 (Murderdean Road). There are indications that pedestrians currently cross the A7 in an uncontrolled manner, as evidenced by an informal “cut-through” to the supermarket car park on the village side of the A7. A new signal controlled pedestrian crossing is proposed on Murderdean Road.

Anecdotal evidence suggests the 30mph (50kph) speed limit in force on the A7 at this location is regularly exceeded. Therefore the new crossing has been positioned to suit a design speed of 60kph (37.5mph). Forward visibility to the crossing for southbound traffic on the A7 is approximately 95m from the crest of the railway bridge, which exceeds the 90m desirable minimum stopping sight distance for a design speed of 60kph. Additional measures such as advanced warning signs and road markings would help to reduce speeds. The new crossing would formalise and reinforce the existing pedestrian desire line, with obvious benefits in terms of improved safety and enhanced permeability.

Midlothian Council’s Active Travel Strategy aspirations for Newtongrange envisages an extended network of cycle and pedestrian routes throughout the village. The new crossing will provide the key connectivity across the A7, allowing the network to operate as a coherent whole rather than in disjointed sections.



7.2 DESIGN GUIDANCE

Newtongrange has a unique character, stemming from the industrial mining history and the specific buildings and urban form which supported it. Proposals for the village centre must integrate with and enhance the character and heritage, through their materials, massing and detailing. The guidance below is set out for residential, non-residential and public buildings and also covers materials and the design of the public realm.

HOUSING

Existing housing in and adjacent to the masterplanning area have the following built form qualities:

- Terraced housing
- Strong frontage
- Straight building lines to housing,
- Broken roof lines
- Eaves parallel to the street
- Dormers / chimneys creating interesting roofscape.
- All pitched roofs in traditional slate
- All housing in red brick
- Small front gardens
- Back lanes incorporating parking.

Any new housing should be in keeping with the scale, form and materials of the village.

A broken roofline is appropriate, with eaves detail a contemporary reworking of traditional overhanging eaves with exposed timber joists. Flat roofs and large gables addressing the High Street are not appropriate.

Long unbroken eaves and blank expanses of wall are to be avoided, as they visually dominate the street scene.

Appropriate materials would be a simple palette of natural materials, contrasting and complementing the existing predominant red brick of the village. If red brick is proposed, it should be carefully selected and detailed so as not to compete with



Predominately terraced housing



Strong frontage with eaves parallel to street

the existing built environment, or seem like a match. Mortar colour, brick tone and texture can all be used to differentiate the old with the new, and could create an interesting and attractive juxtaposition.

While this report does not aim to prescribe a design solution for the site, the distinctive terraced housing with small front gardens and larger rear gardens, and parking tucked into rear lanes, is a compelling and successful solution to retain high density low rise housing in the centre of the village. This urban form could be explored in any proposals for the site.

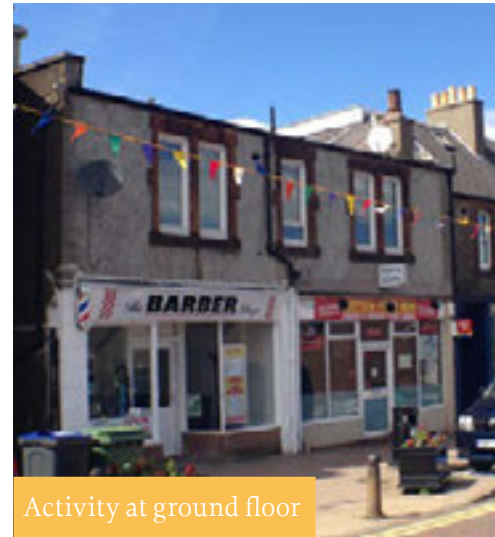
NON RESIDENTIAL USES

New non-residential buildings are important for the life of the village, bringing activity and strengthening the community of Newtongrange. New commercial and retail units in the site should follow the principles set out below:

- Any new buildings should be in keeping with the scale, form and materials of the village;
- Activity needs to be concentrated at ground floor;
- Residential uses should be provided above commercial where practical, with a separate access from street level;
- Signage to follow Midlothian Council guidance and be in keeping with the character of the village;
- In certain areas, especially around the proposed square, areas for outdoor tables should be considered;
- Parking to be provided in the proposed central square or where practical, adjacent to shops;
- Materials should predominantly reflect the traditional mix of red brick and slate, however in certain instances stone and render might also be acceptable.

Frontage to Main Street is an important characteristic of Newtongrange, with shopfronts stepping slightly along the street and housing presenting a strong building line with a stepped roofline adding interest to the traditional terrace of shops. New units and public buildings should prioritise shopfronts and entrances to Main Street and the new square.

Any proposals should carefully consider how to address Main Street, and incorporate elements of the existing character of Newtongrange. Active frontage should be present along the street, with no blank gables to the primary route.



Activity at ground floor



Signage and shop-fronts



Slate roofs

PUBLIC BUILDINGS

Community use buildings should be architecturally distinct as landmarks along Main Street, through their materials, design and relationship with the public realm. It would be appropriate for community uses to engage actively with the street, through large areas of glazing and flexible frontage allowing activity to spill out onto the street and the proposed new square.

Entrances should be available from Main Street and the new square and be welcoming, with spaces of a suitable scale and size for the anticipated audience.

Parking should be provide adjacent to the public buildings and be well overlooked and integrated into the public realm.

MATERIALS AND DETAILING

Materials and detailing should not be slavishly copied, but should inspire the design of proposals. Simple forms, massing and roofscapes which are similar in scale to the existing would be appropriate, rather than flat roof, gable fronts or parapets.



Parking adjacent to shops



Corner treatment

PUBLIC REALM

Accessible and well connected streets and spaces allow people to move around easily and safely, and are full of activity. The centre of the village is more than a street to get from A to B, it is a social space and offers a sense of community. Spaces and connections should be easy to find and navigate, and should be welcoming to residents and visitors.

Recent works to the Main Street at the southern end to the junction with Fifth Street finished the road in a mix of rolled chip tarmac with stone paviors to parking areas, with a mix of square paviors and buff concrete paving to the footways. This smart and robust palette of materials allows the roadway to be visually reduced, and parking areas are clearly demarked. The character of this section of road is congruent with the village's scale and character. Proposals should follow this exemplar.

Proposed materials should be of a similar, simple palette. It would be appropriate for the primary carriageway to continue in a rolled chip tarmac. Most areas of the new public realm should be designed to preclude car access where this isn't appropriate. Surfacing detail can be used to indicate this.

A simple palette of two or three materials for the paving of the new square would be more appropriate than the current mix. Paving materials should be appropriate for the conservation area and relate well to recent works on Main Street.



Plant beds



Public realm treatment and street trees



Mixture of paving materials

Consideration should be given to integrating Sustainable Drainage Solutions (SuDS) where possible, to reduce the reliance on sewers and ensure provision is made for climate change.

Accessibility is the highest priority; materials should be safe for residents and visitors of all ages and abilities, and should aid wayfinding. Kerb heights and bus stop locations should be integrated with the existing streets to maximise accessibility.

Street furniture should be of a high quality, attractive and suitable for the conservation area, complementing the built form. Benches and places to gather should be well overlooked and located close to bus stops and amenities. Strictly functional bollards etc. should be avoided. Delineation of vehicle-free zones can be managed through kerbs, planting and other subtle measures.

Planting and soft landscaping should be integrated with the built environment, not added on. The existing raised beds with formal flower beds and street trees within tree pits are a precedent for the style and scale of landscaping which is appropriate for the area.

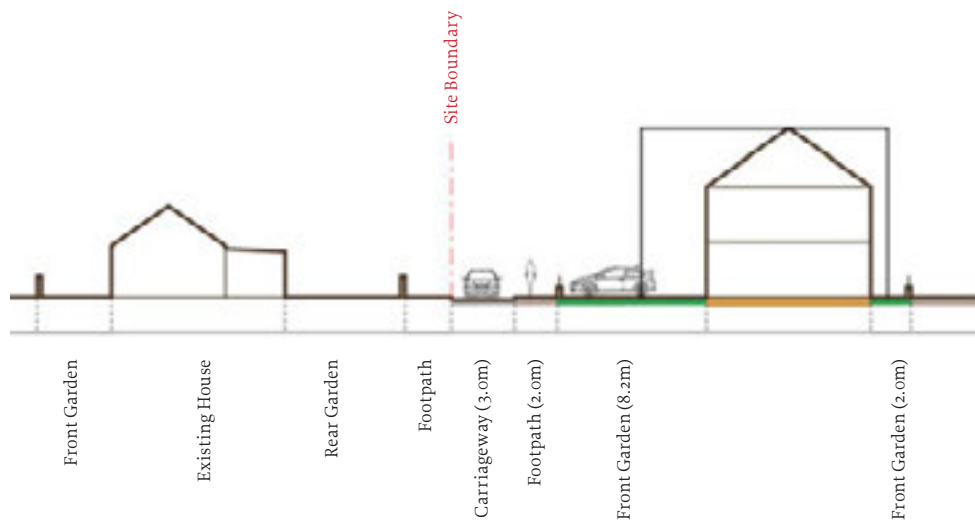
Well kept flower beds in raised planters on red brick are a feature of the recently upgraded rear lanes between First Street and Sixth Street. These can be used to break down the scale of large spaces, or separate pedestrian only areas without the use of bollards.

The monument to the Mineworkers is an important feature, currently located in front of the Library. This should be retained or relocated in a similarly conspicuous location, preferably in the proposed square.

7.3 EDGE TREATMENT

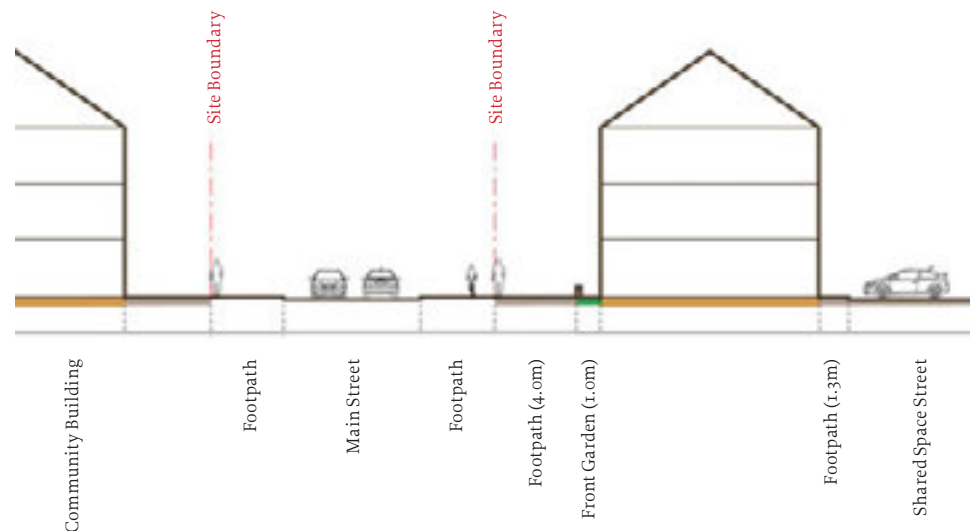
The following pages set out a number of sections to illustrate recommended approaches to treatment of edges with existing areas in the village centre - noting issues such as massing, frontage, setback, boundary treatment, tree planting, parking and relationship to existing roads and housing. These are based on the Aspirational option but are equally applicable to other layouts.

Whilst there are opportunities for some flexibility and varied solutions, the following provide guidance on these various issues. These both reflect good urban design practice and consideration of local context.



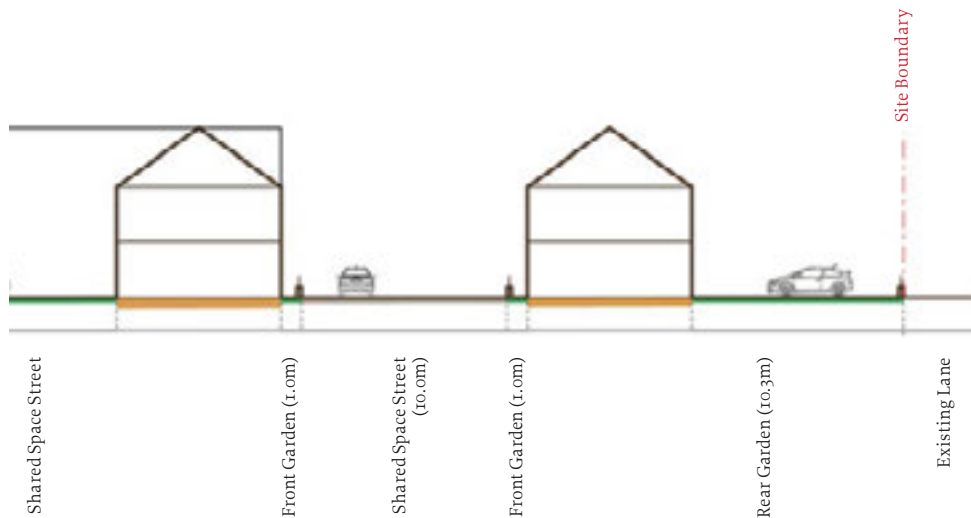
Adjacent to existing back-gardens

- Where existing houses back on to the site, it's best to mirror this by providing backs or sides of housing, ensuring appropriate setback is maintained from existing housing
- Setback may be provided with carriageway, footpath or garden
- Adjacent to 1-storey houses proposed to be max. 2-storey
- Boundary treatment to match existing, i.e. low wall with fence on top
- In some instances vehicular access and parking may be appropriate from the rear



Relationship with Main Street

- Community, retail and residential buildings must address Main Street
- Taller buildings, up to 3-storey appropriate as per coding plan
- Provide strong, active frontage overlooking Main Street
- Activity at ground floor with access from Main Street where possible
- May be appropriate to provide street tree planting in the public realm
- Ensure defensible space to residential units i.e. min. 1.0m front garden
- Boundary treatment to match existing, i.e. low wall with fence on top
- Apart from visitor spaces in bays and village square, parking should be provided behind residential and community buildings
- Opportunity to provide wider pavements along Main Street
- Community use buildings may be architecturally distinct, with opportunity for new Church Centre and Community Hub to become a local landmark



Internal lanes & Parking Courtyard Treatment

- Boundary treatment to match existing, i.e. low wall with fence on top
- Where no interface with existing areas, a variation of edge treatment could be appropriate
- Parking courts may be provided with a shared space approach
- Where possible planting or small trees can be used to enliven parking areas
- On-plot parking may be provided, strong boundaries must still be provided
- Parking should be close and convenient to residential units
- Parking should be well-overlooked
- Back-to-back gardens with a lane between, as elsewhere, may be appropriate



Adjacent to existing front gardens

- The translation between new development and the adjacent 1 storey cottages should be carefully considered.
- Front gardens should always be provided with a min width of 1m
- Boundary treatment to match existing, i.e. low wall with fence on top
- Back gardens should never front onto existing streets



8

PHASING

8.0 PHASING

8.1 PHASING

■ The proposed phasing strategy presented in this section shown by the following diagrams illustrates the key principles for phasing and the broad direction of development and infrastructure construction. This phasing is indicative only and is intended to demonstrate the principle of delivering community benefit early in the development process. The phasing process should seek to ensure continuity of community services at the heart of Newtongrange and the creation of a public space as a priority. An early commitment to placemaking will create momentum and value.

PHASE 1

It is assumed that the first phase will involve the demolition of the church and church hall and construction of a new Church Centre on site, which would provide a place for worship and space for other events. The remaining part of the church site would be developed for a mixture of residential units.

PHASE 2

The second phase would deliver the Community Hub and a portion of the village square, and would require the demolition of the swimming pool and leisure centre. Recognising that this creates cash flow issues, it would also include small elements of housing to east and north of the Hub. Rationalising the existing services would allow for other land parcels to be released for development. This would be followed by the creation of a further area of the public realm at the new square, which would include further areas of car parking, adjacent to new retail units.

PHASE 3

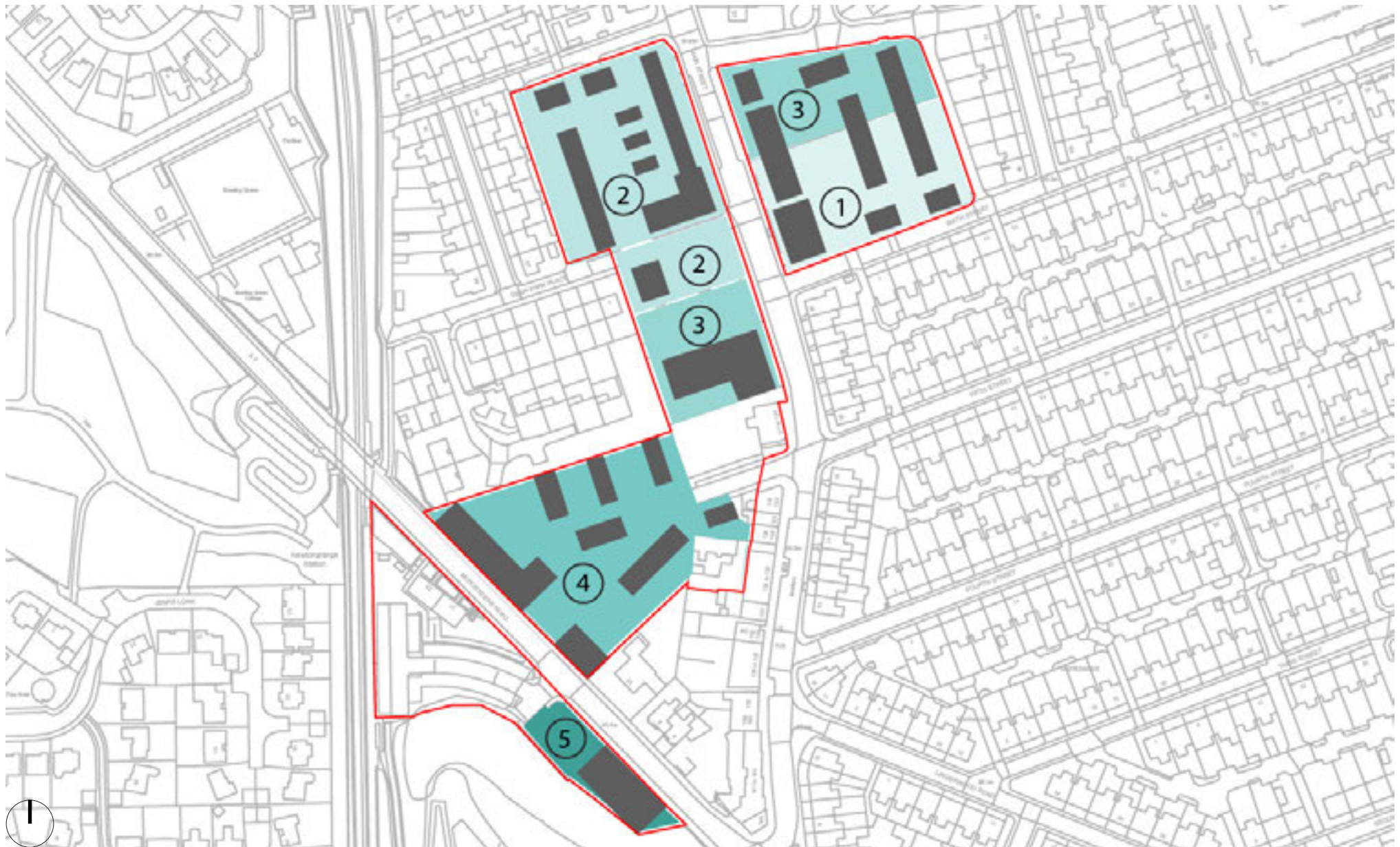
Phase 3 would involve the demolition of the library building which by then will be relocated to the new Hub. This phase would include the development of the gap site on main street, to create a new retail unit at ground floor with apartments above. This will be supplemented by a further area of public realm and new parking facilities.

PHASE 4

Phase 4 provides a redevelopment of the Brotstown Developments site into a residential neighbourhood, combining a mixture of sizes and tenure. A new connection would also be established to Murderdean Road and the station.

PHASE 5

Phase 5 includes the construction of new office space with associated parking and servicing on the site of the Old Pay House. The location of this land means that this can be delivered separately from the other land and at an earlier stage in the development of the new village centre.



- MASTERPLAN AREA
- PHASE 1
- PHASE 2
- PHASE 3

- PHASE 4
- PHASE 5
- PREFERRED OPTION LAYOUT

9

ECONOMIC APPRAISAL



9.0 ECONOMIC APPRAISAL

Biggar Economics have been commissioned to carry out an economic impact assessment of proposed investments at Lady Victoria, Stobhill and the Town Centre. These assessments are available separately from Midlothian Council. The potential economic impacts associated with investments at Newtongrange Town Centre consist mainly of:

- the impact associated with the development and construction;
- the impact associated with the operation of the business units;
- the spending associated with new residents;
- the impact associated with the operation of the new retail units;

The main measures of impact are jobs and economic output (measured in terms of Gross Value Added, GVA). For the purposes of the economic assessment 'public' goods, which would be paid for by Midlothian Council or other public sector organisations were excluded. It has been assumed that if they were not built in Newtongrange that the same facility would be constructed elsewhere in Midlothian. So people employed, for example, in the community hub or library would not be additional to a situation where there was no development as anticipated in the Masterplan. The Church Centre was also excluded as there is currently a Church/Church Hall on the site, suggesting that the impact will be little different from the baseline. Furthermore, the Church Centre is likely to be run on a volunteer bases and is therefore likely to have a relatively small economic impact. Although these aspects have been excluded from the analysis they will nonetheless contribute to Newtongrange by providing local social, recreational and health opportunities and may also be used for other purposes, subject to demand.

The outputs of the assessment have been summarised by Biggar according to operational impact and construction impact. Operational impacts include impact of businesses and jobs created and additional household spending. Construction impacts consider uplift to GVA arising from the construction and development process.

In total, adding together the net operational economic impacts associated with the Town Centre Development, it has been estimated that they could generate £4.7 million GVA and 111 jobs in Midlothian, and £5.9 million GVA and 132 jobs in Scotland.

Further, it has been estimated that construction and development could generate a further £5.0 million GVA and 75 job years in Midlothian and £13.0 million GVA and 220 job years in Scotland.

10

CONCLUSION

10.0 CONCLUSION

An opportunity exists to regenerate the centre of Newtongrange to establish a lively, attractive and sustainable village centre. This report sets out principles and a vision to how the regeneration of the centre can be implemented and overseen.

At the heart of the plan is a proposal to focus viable community facilities at the heart of the village to create animation and footfall and to link these strongly to the railway station, the Stobhill masterplan area and the National Mining Museum. These proposals, together with substantial improvements in the public realm, will mean that Newtongrange will have a real heart. Community facilities together with the creation of a new church centre will help increase footfall to support local businesses whilst at the same time creating a place that meets some of the day to day social needs of local people.

Viability and funding is always a challenge for regeneration led projects and this study shows that a careful balance will need to be struck between deliverability and aspiration. However the Newtongrange Masterplan proposals benefit from the existence of a willing development partner in the Church and the fact that much of the land is in Council ownership. Value generating development can help to support regeneration proposals and there is scope to work positively with other landowners and businesses within the masterplan area. A carefully orchestrated placemaking process driven by public sector organisations, Midlothian Council, the Community and other public sector organisations including Scottish Government and Blueprint, will create social and economic value to the benefit of the whole village. This process will require strong buy-in and leadership and, with these in place, has every chance of success.

Together with the economic benefits that are beginning to flow from the opening of the Borders Railway, the proposals in this plan set out a route map towards a revitalisation of the heart of this strong and proud community.

APPENDICES

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APPENDICES

APPENDIX A: CANMORE AND HER SITES

Information taken from PastMap and Canmore (HER Scotland)

CANMORE SITES		DETAILS
1	<p>Newtongrange, 5-11 Murderdean Road</p> <p>ID: 316791</p> <p>https://canmore.org.uk/site/316791/newtongrange-5-11-murderdean-road</p>	<p>Office (period unassigned)</p> <p>Two-storey painted brick building with 6 irregular bays to front and piended slate roof. Single storey extension to east with corrugated iron roof. Corbelled eaves course. Mostly sash and cash windows.</p> <p>Appears on the 1st edition (1:2500 series) OS map. Given the similarities in scale and decorative details, it is likely this building was constructed around the same time as the Category C(s) Listed 21-23 Murderdean Road, for the Dean Oil Works. May originally have been housing, thought to latterly have functioned as offices. The corbelled detail is also utilised within parts of Lady Victoria Colliery.</p>
2	<p>Newtongrange, 23 Murderdean Road (23 Murderdean Road; Lady Victoria Colliery)</p> <p>ID: 261404</p> <p>https://canmore.org.uk/site/261404/newtongrange-23-murderdean-road</p>	<p>Managers House (19th Century)</p> <p>Single storey and basement, mirrored pair of 3-bay, rectangular-plan cottages. Red brick with long and short yellow brick dressings. Projecting cills; segmental-arched bipartite windows; corbelled eaves course. 10-pane timber sash and case windows. Piended grey slate roof with terracotta ridge with terracotta finials to apex. Cast-iron rainwater goods.</p> <p>21 & 23 Murderdean Road were built to house the managers of a nearby oil well, which was owned by the Coal Board. The Lady Victoria Colliery (see separate listing) is adjacent, and the corbelled eaves course is the same as that on some of the colliery sheds. Newtongrange was Scotland's largest mining village. (Historic Scotland)</p>
3	<p>Newtongrange, 21 Murderdean Road (23 Murderdean Road; Lady Victoria Colliery)</p> <p>ID: 174656</p> <p>https://canmore.org.uk/site/174656/newtongrange-21-murderdean-road</p>	<p>Managers House (19th Century)</p> <p>Single storey and basement, mirrored pair of 3-bay, rectangular-plan cottages. Red brick with long and short yellow brick dressings. Projecting cills; segmental-arched bipartite windows; corbelled eaves course. 10-pane timber sash and case windows. Piended grey slate roof with terracotta ridge with terracotta finials to apex. Cast-iron rainwater goods.</p> <p>21 & 23 Murderdean Road were built to house the managers of a nearby oil well, which was owned by the Coal Board. The Lady Victoria Colliery (see separate listing) is adjacent, and the corbelled eaves course is the same as that on some of the colliery sheds. Newtongrange was Scotland's largest mining village. (Historic Scotland)</p>
4	<p>Newtongrange, Murderdean Road, Dean Oil Works (Newtongrange Station)</p> <p>ID: 351478</p> <p>https://canmore.org.uk/site/351478/newtongrange-murderdean-road-dean-oil-works</p>	<p>Oil Works (19th Century)</p> <p>Standing Building Recording (21 August 2014 - 22 August 2014)</p> <p>Standard basic building survey on a single storey garage. This was a brick-built structure created from the reduction of an early building associated with the 19th-century Dean Oil Works.</p> <p>For associated buildings see NT36SW 58, NT36SW 186 and NT36SW 244.</p> <p>Information from OASIS ID: cfaarcha1-188228 (M Cressey) 2014</p>

CANMORE SITES		DETAILS
5	<p>Edinburgh To Hawick Branch Railway, Newtongrange Station (The Waverley Line)</p> <p>ID: 349221</p> <p>https://canmore.org.uk/site/349221/edinburgh-to-hawick-branch-railway-newtongrange-station</p>	<p>Road Bridge (Period Unassigned)</p> <p>No information available</p>
6	<p>Newtongrange, Main Street, Newtongrange Parish Church Hall</p> <p>ID: 254508</p> <p>https://canmore.org.uk/site/254508/newtongrange-main-street-newtongrange-parish-church-hall</p>	<p>Church Hall (Period Unassigned)</p> <p>No information available</p>
7	<p>Newtongrange, Main Street, Newtongrange Parish Church (Lady Victoria Colliery)</p> <p>ID: 174667</p> <p>https://canmore.org.uk/site/174667/newtongrange-main-street-newtongrange-parish-church</p>	<p>Church (Period Unassigned)</p> <p>No information available</p>

CANMORE SITES		DETAILS
8	<p>Newtongrange, Seventh Street, Proposed Temporary Surgery For A R Acton</p> <p>ID: 213059</p> <p>https://canmore.org.uk/site/213059/newtongrange-seventh-street-proposed-temporary-surgery-for-a-r-acton</p>	<p>No classification</p> <p>No information available</p>
9	<p>Newtongrange, Main Street, Miners' Memorial</p> <p>ID: 290821</p> <p>https://canmore.org.uk/site/290821/newtongrange-main-street-miners-memorial</p>	<p>Commemorative Monument (Period Unassigned)</p> <p>No information available</p>
10	<p>Newtongrange, 113 Main Street, Newbattle Swimming Pool</p> <p>ID: 212667</p> <p>https://canmore.org.uk/site/212667/newtongrange-113-main-street-newbattle-swimming-pool</p>	<p>Swimming Pool (Period Unassigned)</p> <p>No information available</p>

HER SITES (information from John Gray Centre)		DETAILS
1	<p>Newtongrange, Main Street, Newtongrange Parish Church Hall</p> <p>HER Number: MEL6926</p> <p>http://www.johngraycentre.org/collections/getrecord/MLHER_MEL6926/</p>	<p>Historic Scotland Listed Building Description: A Murray Hardie, 1939-1942. Modern, harled church with traditional details; crowstepped bell tower; dormer windows; flying buttresses; deep sloping green slate roof. Long and short concrete surrounds to all but dormer and N transept windows; concrete base course.</p>
2	<p>Newtongrange, Main Street, Newtongrange Parish Church</p> <p>HER number: MEL5136</p> <p>http://www.johngraycentre.org/collections/getrecord/MLHER_MEL5136/</p>	<p>Historic Scotland Listed Building Description: A Murray Hardie, 1939-1942. Modern, harled church with traditional details; crowstepped bell tower; dormer windows; flying buttresses; deep sloping green slate roof. Long and short concrete surrounds to all but dormer and N transept windows; concrete base course.</p>
3	<p>Newtongrange, Main Street, Miners' Memorial</p> <p>HER number: MEL9647</p> <p>http://www.johngraycentre.org/collections/getrecord/MLHER_MEL9647/</p>	<p>Commemorative Monument</p> <p>No information available</p>
4	<p>Newtongrange, Seventh Street, Proposed Temporary Surgery For A R Acton</p> <p>HER number: MEL5780</p> <p>http://www.johngraycentre.org/collections/getrecord/MLHER_MEL5780/</p>	<p>Building</p> <p>No information available</p>

HER SITES (information from John Gray Centre)		DETAILS
5	<p>Newtongrange, 113 Main Street, Newbattle Swimming Pool</p> <p>HER number: MEL5694</p> <p>http://www.johngraycentre.org/collections/getrecord/MLHER_MEL5694/</p>	<p>Swimming Pool</p> <p>No information available</p>
6	<p>Newtongrange, 5-11 Murderdean Road</p> <p>HER number: MEL10798</p> <p>http://www.johngraycentre.org/collections/getrecord/MLHER_MEL10798/</p>	<p>Office</p> <p>Description: Two-storey painted brick building with 6 irregular bays to front and piended slate roof. Single storey extension to east with corrugated iron roof. Corbelled eaves course. Mostly sash and cash windows.</p> <p>Appears on the 1st edition (1:2;500 series) OS map. Given the similarities in scale and decorative details, it is likely this building was constructed around the same time as the Category C(s) Listed 21-23 Murderdean Road, for the Dean Oil Works. May originally have been housing, thought to latterly have functioned as offices. The corbelled detail is also utilised within parts of Lady Victoria Colliery.</p>
7	<p>Newtongrange, 23 Murderdean Road</p> <p>HER number: MEL7169</p> <p>http://www.johngraycentre.org/collections/getrecord/MLHER_MEL7169/</p>	<p>Building</p> <p>Historic Scotland Listed Building Description: Late 19th century. Single storey and basement, mirrored pair of 3-bay, rectangular-plan cottages. Red brick with long and short yellow brick dressings. Projecting cills; segmental-arched bipartite windows; corbelled eaves course.</p>
8	<p>Newtongrange, 21 Murderdean Road</p> <p>HER number: MEL5132</p> <p>http://www.johngraycentre.org/collections/getrecord/MLHER_MEL5132/</p>	<p>House, late 19th century</p> <p>Historic Scotland Listed Building Description: Late 19th century. Single storey and basement, mirrored pair of 3-bay, rectangular-plan cottages. Red brick with long and short yellow brick dressings. Projecting cills; segmental-arched bipartite windows; corbelled eaves course.</p>

APPENDIX B: SUB-AREA OPTION REVIEW

The following pages set out the options considered for each of the sub-areas, which helped inform the design process. The sub areas are shown in the diagram right. The options presented include both discarded and those taken forwarded as part of a preferred option.



1 SUB-AREA DIAGRAM

- 1 CHURCH/LIBRARY SITE
- 2 COMMUNITY SITE
- 3 BROTSTOWN DEVELOPMENT SITE
- 4 STATION SITE

CHURCH/LIBRARY

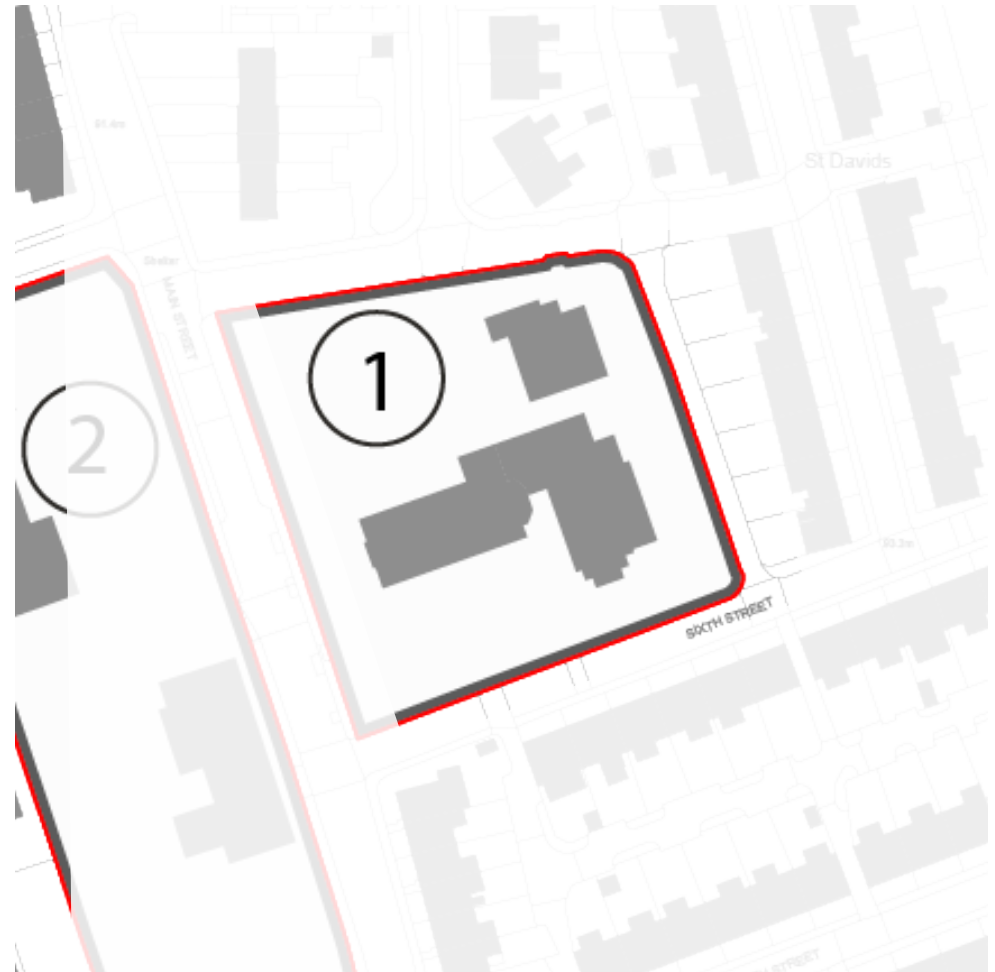
The first option developed for this area was based on retaining the Parish Church building but demolishing the church hall and library buildings. Whilst allowing for retaining the church building was seen as a positive in townscape terms, due to the building acting as a landmark in the village, this option also constrained the possibilities, both due to the high costs of the conversion of the existing church and relatively low number of potential dwellings that could be accommodated, partly due to the siting of the existing church. This option delivers only around 38 dwellings of predominantly smaller sizes.

The relationship to adjacent existing 1-storey houses to the south and east in particular requires an appropriate relationship with any proposed development. The west edge addresses Main Street, which suggests a requirement for a strong frontage. As a small site, bounded by four roads and with the listed church, this is a tightly constrained option.

A second option was developed on the basis of a complete clearance of the site and redevelopment into a residential area with a new Church Centre on Main Street. This option delivers about 35 new, mostly larger dwellings, and a new Church Centre of 340sqm.

Whilst the site is still constrained in many of the same ways, the provision of a New Church Centre in a new position, gives greater flexibility giving a much bigger proportion of 3-bed houses than in the first option.

The second option was the preferred option taken forward.



COMMUNITY SITE

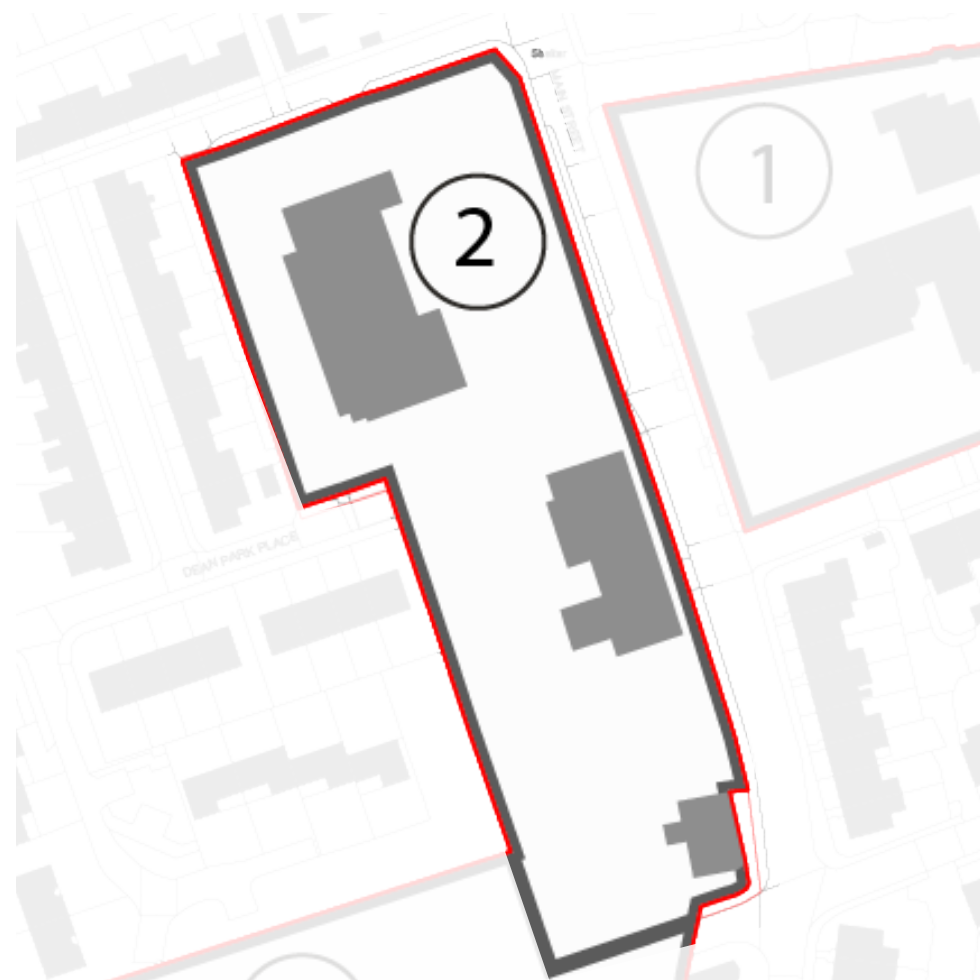
A first option was developed on the basis of a complete clearance of the site, including the swimming pool and the leisure centre, and redevelopment into a mixed use area, combining retail, community and residential use, centred around a new village square. This option delivered 24 new flats and houses and a new Community Hub of 1,210sqm, as well as new retail space.

Important considerations were the relationship with existing dwellings to the north and west of the site, plus providing frontage to Main Street with community and retail uses.

The second option was developed on the basis of retaining the Newbattle Pool and demolishing the leisure centre. A community hub building would then be constructed adjacent to the swimming pool and with some retail and residential units, enclosing a new village square. This option delivered 14 new flats and houses and a new Community Hub of 1,210sqm, as well as new retail space.

The retention of the Pool in its existing location restricted the provision of residential units significantly.

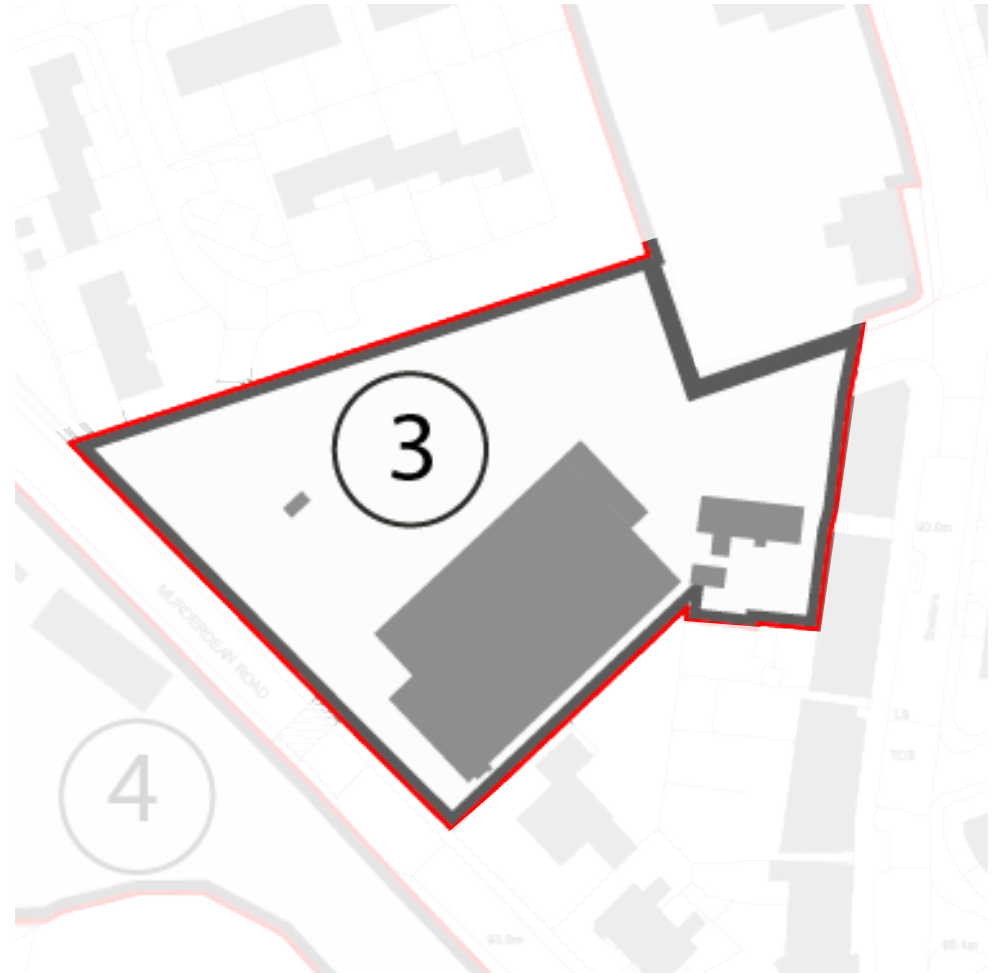
The first option was the preferred option taken forward. However this was modified substantially through design development in light of viability testing to maximise values and reduce costs.



BROTSTOWN DEVELOPMENT SITE

Options considered for this site were partial and complete redevelopment of the site. The latter was preferred due to the greater opportunity to provide improvements to the area, including a new pedestrian connection to the station.

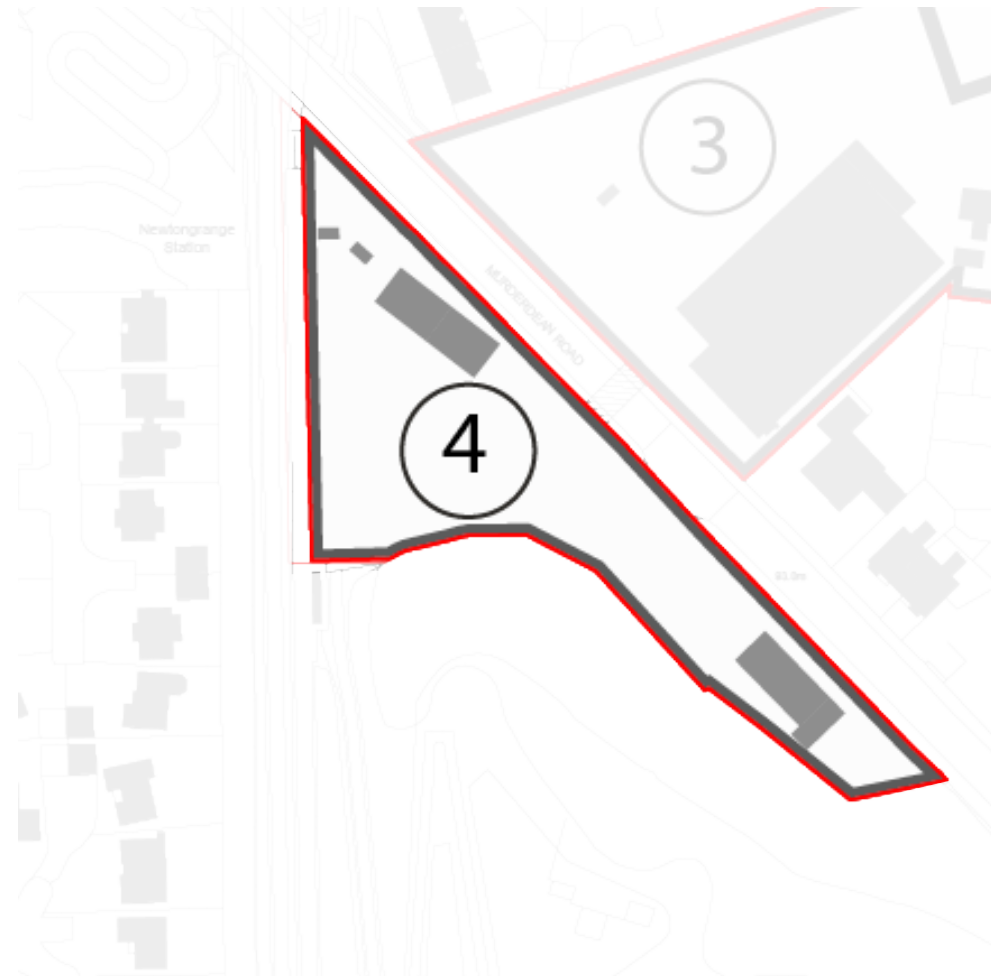
Important considerations were the relationship with existing residential properties to the north, and residential/retail to the south and east. It is also important to address the relationship with Murderdean Road and provide the opportunity for a pedestrian route from the station to the village centre.



STATION SITE

This option was developed on the basis of the demolition of the old Pay House and creation of a new office building of approximately 1,000 sqm spread across two storeys, with associated parking and service areas.

The option considered a frontage to Murderdean Road and provision of carparking easily accessible from the station entrance to the north.



LDĀ DESIGN

London

^A New Fetter Place
8 - 10 New Fetter Lane
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